



**Illinois Department
of Transportation**

Highway Permit

District Serial No. _____

042
Whereas, I (We) _____ City of Decatur _____ #1 Gary K. Anderson Plaza _____
(Name of Applicant) (Mailing Address)

_____ Decatur _____ Illinois 62523 _____ hereinafter termed the Applicant,
(City) (State)
request permission and authority to do certain work herein described on the right-of-way of the State Highway
known as _____ FAP 716 _____ Route _____ ILL. 48 _____, Section _____ 130 _____,
from Station _____ 48 + 51 _____ to Station _____ 51 + 47 _____
_____ Macon _____ County. The work is described in detail on the attached plan or sketch and/or as follows:

-----SEE ATTACHED SHEET FOR SPECIAL PROVISIONS-----

All work authorized by this permit shall be completed _____ 180 days _____ after the date this permit is approved,
otherwise the permit becomes null and void.

This permit is subject to the conditions and restrictions printed on the reverse side of this sheet.

This permit is hereby accepted, and its provisions agreed to this _____ 13 _____ day of _____ July _____, 2020

Witness _____
_____ 1 Gary K. Anderson Plaza _____
_____ Decatur _____ IL _____
City State

Signed _____
_____ 1 Gary K. Anderson PL _____
_____ Decatur _____ IL _____
City State
Phone No. _____ 217 - 424 - 2547 _____

SIGN AND RETURN TO: Regional Engineer _____ Jeffrey P. Myers, State Highway Building, Effingham, IL 62401

Approved this _____ day of _____, _____

Department of Transportation

BY: _____
Regional Engineer

First: The Applicant represents and warrants that he/she is the party in interest respecting this Permit and that he/she is the agent in fact with authority to bind all parties in interest to the obligations and undertakings agreed to in this Permit. The Applicant represents and warrants that the property lines shown on the attached plan sheet(s) or sketch are true and correct, and that all proposed work is accurately depicted thereon.

Second: The proposed work shall be located and constructed to the satisfaction of the Regional Engineer or his/her duly authorized representative. No revisions or additions shall be made to the proposed work on the right-of-way without the written permission of the Regional Engineer. The Applicant agrees to complete all work to the standards and specifications identified by the Regional Engineer or his/her authorized representative as a condition of granting this Permit. The Applicant agrees to furnish all labor, equipment and material, and do all work and pay all costs associated with the work authorized by this Permit. The Applicant agrees to restore any and all damaged portions of the highway right-of-way to the condition satisfactory to the Regional Engineer or his/her authorized representative including, but not limited to, all landscape restoration. The Applicant shall not trim, cut or in any way disturb any trees or shrubbery along the highway without the approval of the Regional Engineer or his/her duly authorized representative. Any and all documents, writings and notes reflecting or identifying the standards, specifications, understandings and conditions applicable to the performance of the permitted work required by the Regional Engineer or his/her authorized representative are hereby incorporated into this Permit by reference as though fully set forth herein.

Third: The Applicant shall at all times conduct the work in such a manner as to minimize hazards to vehicular and pedestrian traffic. Traffic controls and work site protection shall be in accordance with the applicable requirements of Part 6 (Temporary Traffic Control) of the Illinois Manual on Uniform Traffic Control Devices and with the traffic control plan if one is required elsewhere in the permit. All signs, barricades, flaggers, etc., required for traffic control shall be furnished by the Applicant. The work may be done on any day except Sunday, New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas Day. Work shall be done only during daylight hours.

Fourth: The work performed by the Applicant is for the bona fide purpose expressed and not for the purpose of, nor will it result in, the parking or servicing of vehicles on the highway right-of-way. Signs located on or overhanging the right-of-way shall be prohibited.

Fifth: The Applicant shall engage in only the proposed work approved herein, and subject to the hazards incident to such activities, assumes all risks associated therewith. The Applicant assumes full and strict liability for the actions of itself, all parties in interest, its agents and employees, contractors, subcontractors and consultants. The Applicant and all parties in interest shall save, defend, hold harmless and indemnify the State of Illinois and each of its officers, agents, employees, invitees and others associated with it from and against any and all suits, claims, actions, losses, injuries, damages, judgments and expenses that are based on, or that arise or are alleged to have arisen out of the performance of the work approved herein, including, but not limited to, any act, willful or intended, or negligence of the Applicant and any party in interest, its agents and employees, contractors, subcontractors and consultants whether at law, in equity or common law. In the event the Applicant or any party in interest fails, neglects, or refuses to comply with any provision of this indemnity, the State of Illinois may take any action necessary to protect itself from liability, including any action to pay, settle, compromise and procure the discharge thereof, in which case the Applicant or any party in interest, jointly and severally, shall be liable and bound unto the State of Illinois for any and all expenses related thereto, including attorney's fees.

Sixth: The State reserves the right to make such changes, additions, repairs and relocations within its statutory limits to the facilities constructed under this permit or their appurtenances on the right-of-way as may at any time be considered necessary to permit the relocation, reconstruction, widening or maintaining of the highway and/or provide proper protection to life and property on or adjacent to the State right-of-way. However, in the event this permit is granted to construct, locate, operate and maintain utility facilities on the State right-of-way, the Applicant, upon written request by the Regional Engineer, shall perform such alterations or change of location of the facilities, without expense to the State, and should the Applicant fail to make satisfactory arrangements to comply with this request within a reasonable time, the State reserves the right to make such alterations or change of location or remove the work, and the Applicant agrees to pay for the cost incurred.

Seventh: This permit is effective only insofar as the Department has jurisdiction and does not presume to release the Applicant from compliance with the provisions of any existing statutes or local regulations relating to the construction of such work.

Eighth: The Construction of access driveways is subject to the regulations listed in the "Policy on Permits for Access Driveways to State Highways." If, in the future, the land use of property served by an access driveway described and constructed in accordance with this permit changes so as to require a higher driveway type as defined in that policy, the owner shall apply for a new permit and bear the costs for such revisions as may be required to conform to the regulations listed in the policy. Utility installations shall be subject to the "Policy on the Accommodation of Utilities on Right-of-Way of the Illinois State Highway System."

Ninth: If the work covered by this permit includes construction of additional lanes, turn lanes, median cross-overs or traffic signals on, along or adjacent to a highway under Department jurisdiction, the permittee shall use only contractor(s) approved by the Department of Transportation for the performance of said work on the State highway. A contractor currently prequalified by the Department in the work rating governing the said work shall be approved. Prior to the commencement of the said work on the State highway, the applicant shall furnish the Regional Engineer a copy of the contractor's current Certificate of Eligibility, or, if the permittee proposes to use a contractor not currently prequalified by the Department, information satisfactory to the Department evidencing the contractor's qualification and ability to perform the said work. No work on the State highway shall be performed until the Department issues an approval of the proposed contractor.

ILLINOIS 48 GEOMETRIC CHANGES FOR A PROPOSED FIRE STATION

The Applicant is hereby granted permission to construct a median break to a fire station and right-in/right-out median entrance. This work also includes geometric changes to Illinois 48, sidewalk, striping and other related work on the State highway right-of-way. This work will be located and constructed in accordance with the Special Provisions, description and plans attached hereto and made a part of this permit. The material used in this permit shall be from a State approved source.

This permit is granted for the construction of a median break on a highway under State jurisdiction. The Applicant shall only utilize contractor(s) prequalified by the Department of Transportation for the performance of required work. Prior to the commencement of the work described in this permit, the Applicant shall furnish the Regional Engineer a copy of the contractor's current Certificate of Eligibility.

All labor and material required to complete and maintain this permit are the responsibility of the Applicant.

The Applicant shall comply with all Environmental Protection Agency (EPA) National Pollutant Discharge Elimination System (NPDES) regulations including, but not limited to, ILR 10 (Construction Site Soil Erosion and Sediment Control) and ILR 40 (Site Maintenance and All Other Storm Water Runoff/Discharge).

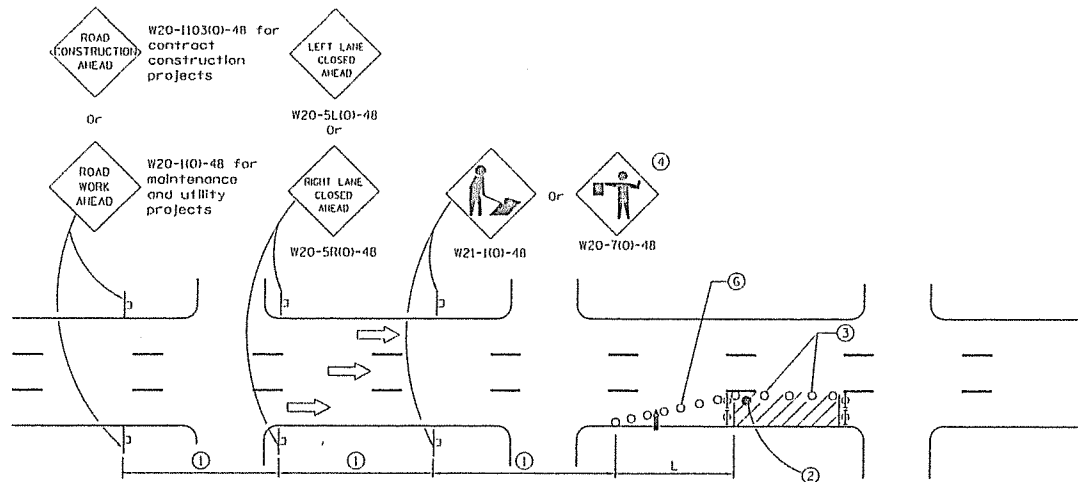
When doing the work covered in this permit, the Applicant shall use Traffic Control Standards 701601, 701006 and 701901 which are attached and made a part of this permit. Standards 701601, 701006, and 701901 are minimum requirements and additional or supplemental devices may be required. All Traffic Control Devices must meet NCHRP-350 requirements.

All work shall be constructed in accordance with the Illinois Department of Transportation Standard Specifications for Road and Bridge Construction adopted April 1, 2016. The Department shall be furnished copies of material and construction inspection to certify compliance with the standard specifications. The Applicant shall furnish enough qualified engineering personnel for construction layout and to insure adequate material and construction inspection to certify compliance with the standard specifications. The Applicant must provide the Department written documentation stating the name of the inspector or firm performing this work, along with documentation of his/her experience for the Department's approval prior to permit execution.

It is understood and agreed that the Applicant, his successors, or assigns shall not alter or change the existing natural drainage contours of this property to discharge additional storm water onto the State highway right-of-way. If a change is proposed in the grade, area, or type of surface, the Applicant, his successors, or assigns shall submit a plan to the Illinois Department of Transportation for review before making any such changes.

A copy of this permit shall be in the possession of the construction foreman on the jobsite for inspection by the State highway personnel.

The Applicant shall contact the Illinois Department of Transportation Operations Office in Effingham (217) 342-3951 a minimum of 48 hours in advance of starting the work so a representative of this office may meet with the construction foreman to discuss the construction details.



SIGN SPACING	
Posted Speed	Sign Spacing
55	500' (150 m)
50-45	350' (100 m)
<45	200' (60 m)

SYMBOLS

- Arrow board
- Cone, drum or barricade
- Sign on portable or permanent support
- Work area
- Barricade or drum with flashing light
- Type III barricade with flashing lights
- Flagger with traffic control sign.

- ① Refer to SIGN SPACING TABLE for distances.
- ② Required for speeds > 40 MPH
- ③ Cones at 25' (8 m) centers for 250' (75 m). Additional cones may be placed at 50' (15 m) centers. When drums or Type I or Type II barricades are used, the interval between devices may be doubled.
- ④ Use flagger sign only when flagger is present.
- ⑤ For approved shoulder closures.
- ⑥ Cones, drums or barricades at 20' (6 m) in taper.

GENERAL NOTES

This Standard is used where at any time, day or night, any vehicle, equipment, workers or their activities encroach on the pavement during shoulder operations or where construction requires lane closures in urban areas.

Calculate L as follows:

SPEED LIMIT	FORMULA	
	English	Metric
40 mph (70 km/h) or less:	$L = \frac{WS^2}{60}$	$L = \frac{WS^2}{150}$
45 mph (80 km/h) or greater:	$L = (WS)$	$L = (0.6WS)$

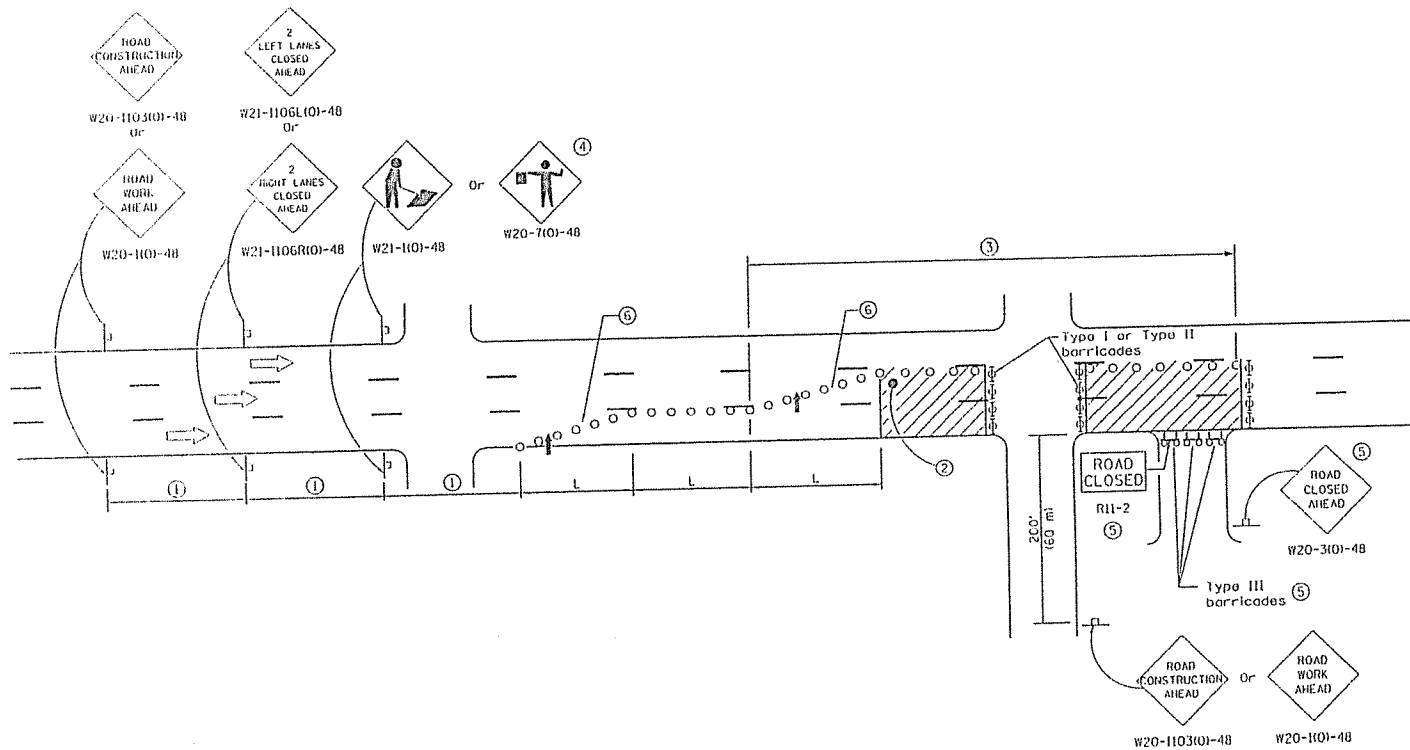
W = Width of offset in feet (meters).

S = Normal posted speed mph (km/h).

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN (Sheet 1 of 2) STANDARD 701601-09
1-1-14	Revised workers sign number to agree with current MUTCD.	
1-1-13	Omitted text 'WORKERS' sign.	

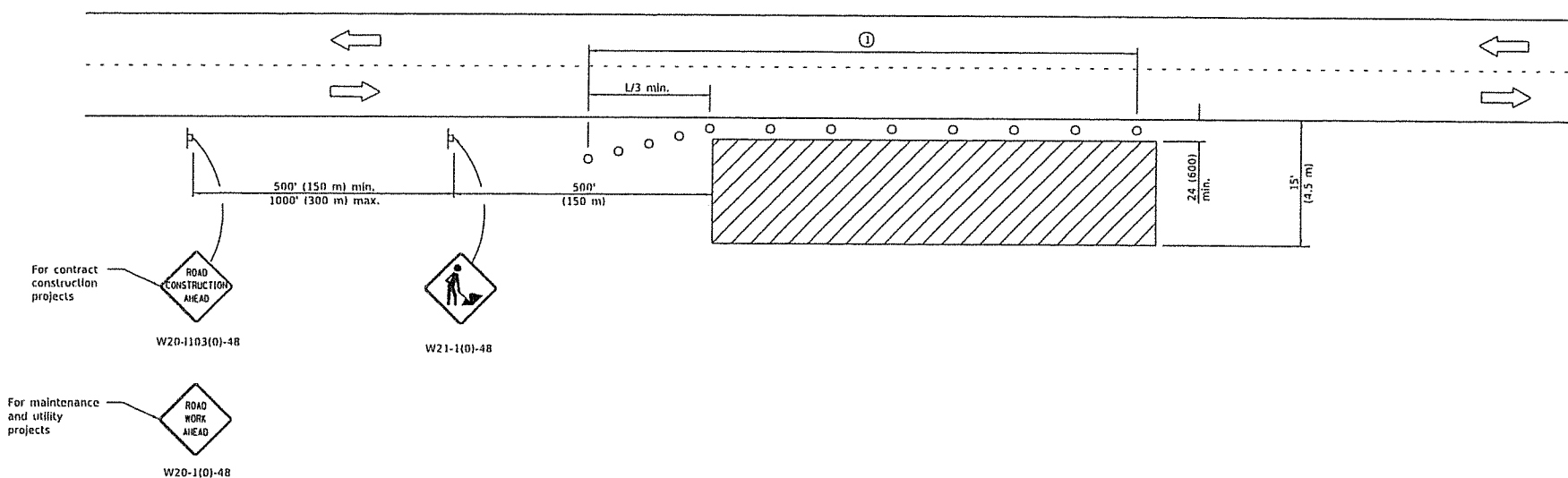
Illinois Department of Transportation	
APPROVED: [Signature] ENGINEER OF SAFETY ENGINEERING APPROVED: [Signature] ENGINEER OF DESIGN AND ENVIRONMENT	1555 1-1-14



**URBAN LANE CLOSURE,
MULTILANE, 1W OR 2W WITH
NONTRAVERSABLE MEDIAN**
(Sheet 2 of 2)

STANDARD 701601-09

Illinois Department of Transportation	
APPROVED	January 1, 2014
ENGINEER OF SAFETY ENGINEERING	
APPROVED	January 1, 2014
ENGINEER OF DESIGN AND CONSTRUCTION	



TYPICAL APPLICATIONS

Utility operations
 Culvert extensions
 Side slope changes
 Guardrail installation and maintenance
 Delineator installation
 Landscaping operations
 Shoulder repair
 Sign installation and maintenance

SYMBOLS

- Work area
- Sign
- Cone, drum or barricade

- ① When the work operation exceeds one hour, cones, drums or barricades shall be placed at 25' (8 m) centers for L/3 distance, and at 50' (15 m) centers through the remainder of the work area.

GENERAL NOTES

This Standard is used where any vehicles, equipment, workers or their activities will encroach in the area 15' (4.5 m) to 24' (600) from the edge of pavement.

Calculate L as follows:

SPEED LIMIT

FORMULAS
 English (Metric)

40 mph (70 km/h)
 or less:

$$L = \frac{WS^2}{60} \quad L = \frac{WS^2}{150}$$

45 mph (80 km/h)
 or greater:

$$L = (W)(S) \quad L = 0.65(W)(S)$$

W = Width of offset
 in feet (meters).

S = Normal posted speed
 mph (km/h).

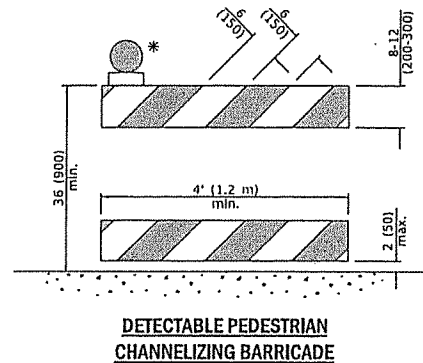
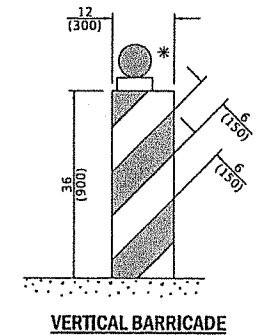
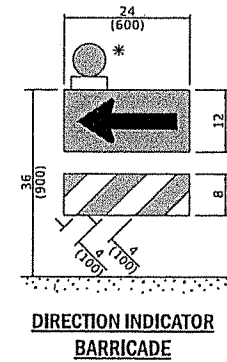
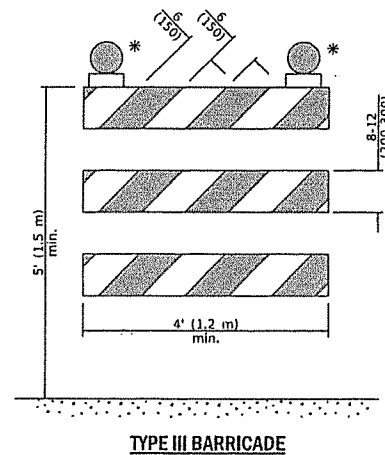
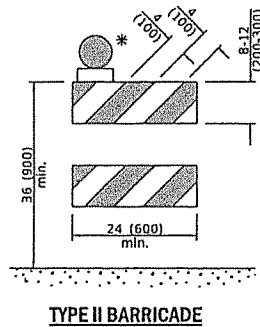
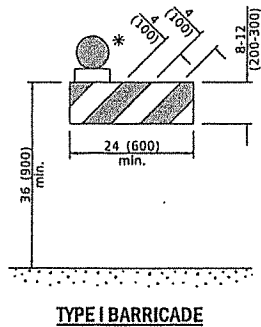
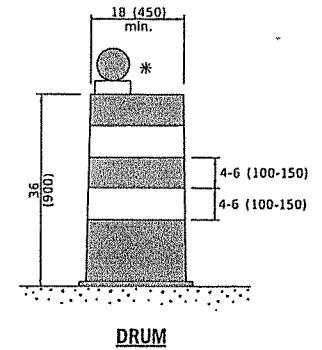
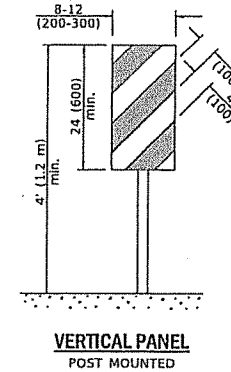
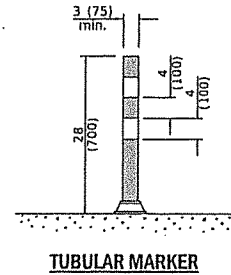
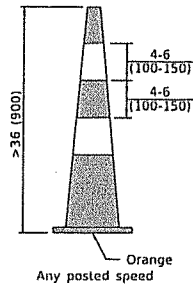
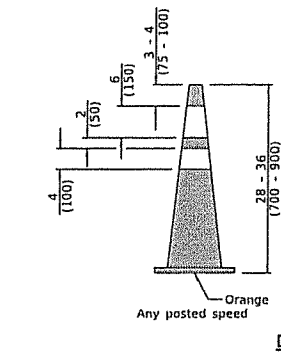
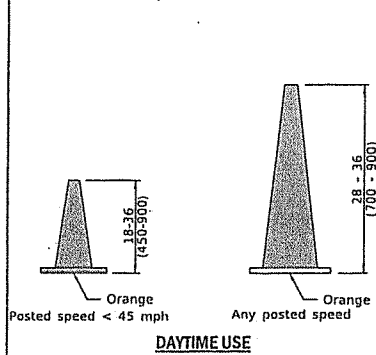
All dimensions are in inches (millimeters)
 unless otherwise shown.

Illinois Department of Transportation	
PASSED	January 1, 2014
ENGINEER OF SAFETY ENGINEERING	
APPROVED	May 1, 2014
ENGINEER OF DESIGN AND ENVIRONMENT	

DATE	REVISIONS
1-1-14	Revised workers sign number to agree with current MUTCD.
1-1-13	Omitted text 'WORKERS' sign.

**OFF-ROAD OPERATIONS, 2L, 2W,
 15' (4.5 m) TO 24" (600 mm)
 FROM PAVEMENT EDGE**

STANDARD 701006-05



* Warning lights (if required)

GENERAL NOTES

All heights shown shall be measured above the pavement surface.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Revised cone usage and added cones > 36" (900 mm) height.
1-1-18	Revised END WORK ZONE SPEED LIMIT sign from orange to white background.

TRAFFIC CONTROL DEVICES

(Sheet 1 of 3)

STANDARD 701901-08

Illinois Department of Transportation	
PROJECT: [REDACTED]	DATE: 7019
ENGINEER OF SAFETY PROG. AND ENGINEERING	
APP: [REDACTED]	019
ENG: [REDACTED]	01-1-19



MAX WIDTH
XX' - XX"
MILES
AHEAD

WIDTH RESTRICTION SIGN

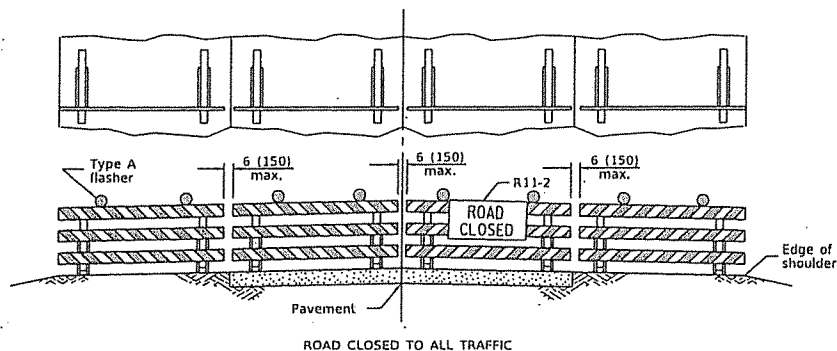
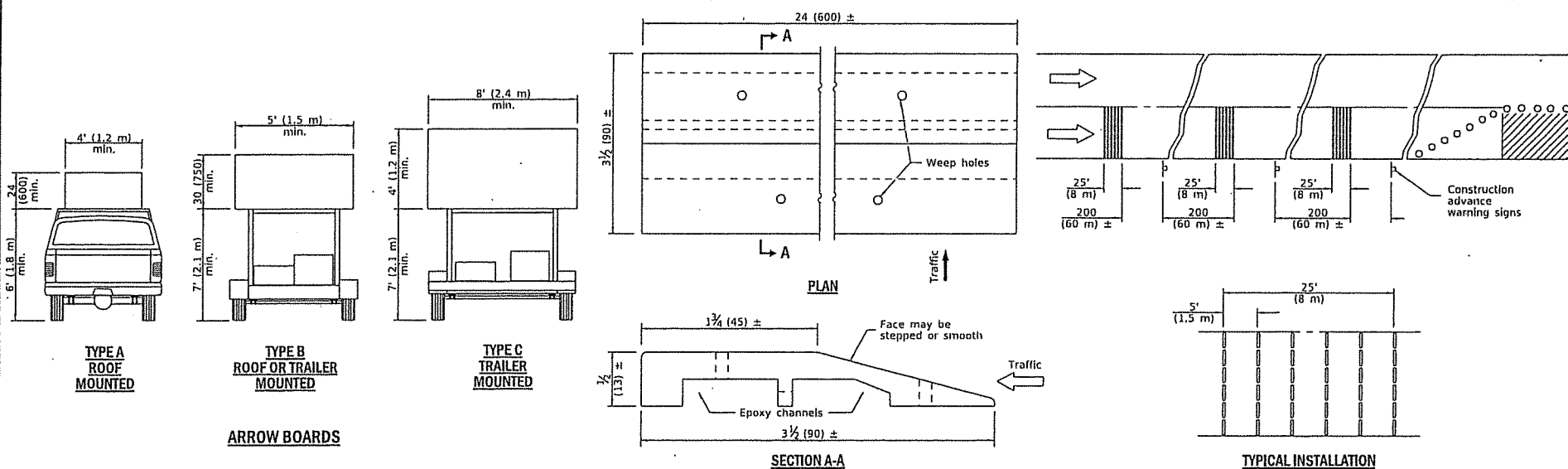
Diagram illustrating the dimensions for a diamond interchange. The approach width is 24 - 10' (600 - 3 m). The approach length is 12 (300) min. The diagram also shows the 'Edge of pavement or face of curb' and the 'Elevation of edge of pavement'.

WORK ZONE
SPEED LIMIT XX
PHOTO ENFORCE

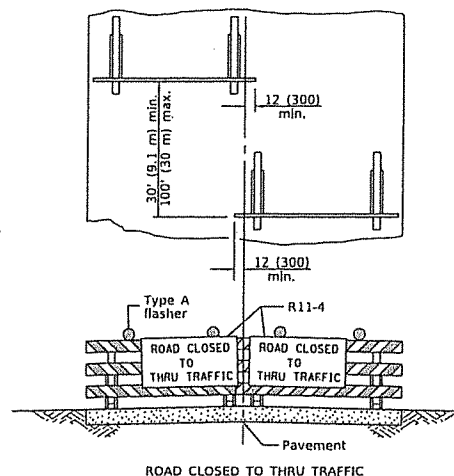
\$XXX FIN MINIMUM

END
WORK ZONE
SPEED LIMIT

STANDARD 701901-08



ReflectORIZED striping may be omitted on the back side of the barricades. If a Type III barricade with an attached sign panel which meets NCHRP 350 is not available, the sign may be mounted on an NCHRP 350 temporary sign support directly in front of the barricade.



ReflectORIZED striping shall appear on both sides of the barricades. If a Type III barricade with an attached sign panel which meets NCHRP 350 is not available, the signs may be mounted on NCHRP 350 temporary sign supports directly in front of the barricade.

Illinois Department of Transportation

2019

ENGINEER OF SAFETY PROG. AND ENGINEERING

APPROVED

2019

ENGINEER OF DESIGN AND ENVIRONMENT

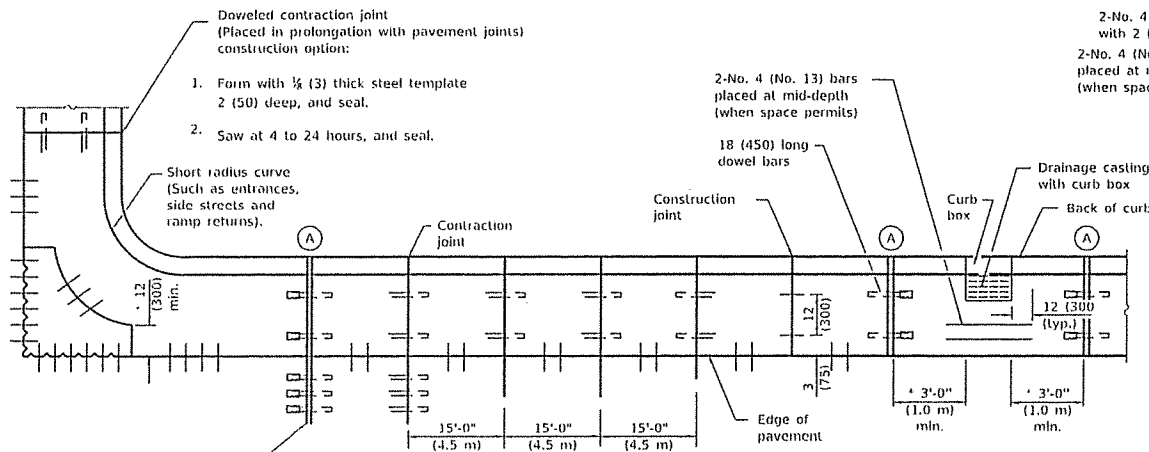
ISSUED 11-13

TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD

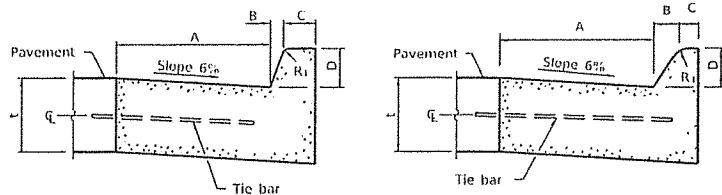
TRAFFIC CONTROL DEVICES

(Sheet 3 of 3)

STANDARD 701901-08



PLAN
ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE



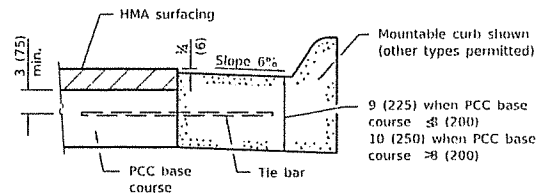
BARRIER CURB

MOUNTABLE CURB

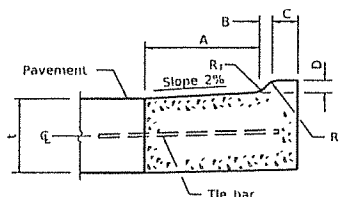
TYPE	A	B	C	D	R
0-6.06 *	6	1	6	6	1
(8-15.15)	(150)	(25)	(150)	(150)	(25)
B-6.12	12	1	6	6	1
(10-15.15)	(300)	(25)	(150)	(150)	(25)
B-6.18	18	1	6	6	1
(10-15.45)	(450)	(25)	(150)	(150)	(25)
B-6.24	24	1	6	6	1
(10-15.60)	(600)	(25)	(150)	(150)	(25)
B-9.12	12	2	5	9	1
(12-22.30)	(300)	(50)	(125)	(225)	(25)
B-9.18	18	2	5	9	1
(12-22.45)	(450)	(50)	(125)	(225)	(25)
B-9.24	24	2	5	9	1
(12-22.60)	(600)	(50)	(125)	(225)	(25)

* For corner islands only.

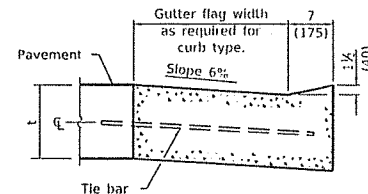
TYPE	A	B	C	D	R ₁	R ₂
M-2.06	6	2	4	2	3	2
(M-5.15)	(150)	50	(100)	(50)	(75)	(50)
M-2.12	12	2	4	2	3	2
(M-5.30)	(300)	(50)	(100)	(50)	(75)	(50)
M-4.06	6	4	3	4	3	NA
(M-10.15)	(150)	(100)	(75)	(100)	(75)	NA
M-4.12	12	4	3	4	3	NA
(M-10.30)	(300)	(100)	(75)	(100)	(75)	NA
M-15.15	15	5	5	5	5	NA
(M-15.45)	(450)	(100)	(75)	(100)	(75)	NA
M-4.24	24	3	4	3	4	NA
(M-10.60)	(600)	(100)	(75)	(100)	(75)	NA
M-6.06	6	6	2	6	2	NA
(M-15.15)	(150)	(150)	(50)	(150)	(50)	NA
M-6.12	12	6	2	6	2	NA
(M-15.30)	(300)	(150)	(50)	(150)	(50)	NA
M-4.18	18	6	6	6	6	NA
(M-13.45)	(450)	(120)	(50)	(150)	(50)	NA
M-6.24	24	6	6	6	6	NA
(M-15.60)	(600)	(150)	(50)	(150)	(50)	NA



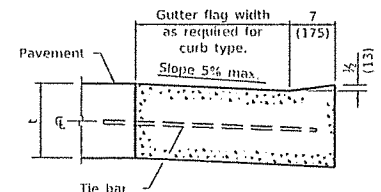
ADJACENT TO PCC BASE COURSE
WITH HMA SURFACING



M-2.06 (M-5.15) and M-2.12 (M-5.30)



DEPRESSED CURB (TYPICAL)



DEPRESSED CURB ADJACENT
TO CURB RAMP ACCESSIBLE
TO THE DISABLED

GENERAL NOTES

The bottom slope of combination curb and gutter constructed adjacent to pcc pavement shall be the same slope as the subbase or 6% when subbase is omitted.

t = Thickness of pavement.

Longitudinal joint tie bars shall be No. 6 (No. 19) at 36 (900) centers in accordance with details for longitudinal construction joint shown on Standard 420001.

A minimum clearance of 2 (50) between the end of the tie bar and the back of the curb shall be maintained.

The dowel bars shown in contraction joints will only be required for monolithic construction.

See Standard 606301 for details of corner Islands.

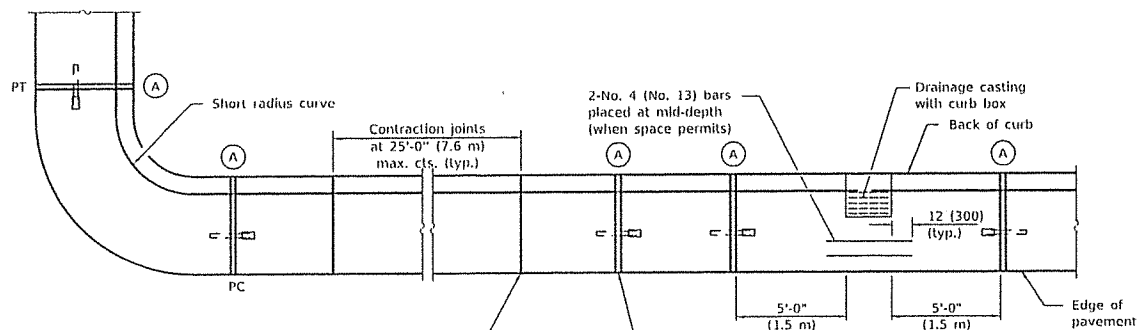
All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-18	Revised General Note for tie bar spacing to 36 (900) cts.
1-1-15	Added B-6.06 (B-15.15) barrier curb and gutter to table (corner Islands only).

**CONCRETE CURB TYPE B
AND COMBINATION
CONCRETE CURB AND GUTTER**

(Sheet 1 of 2)

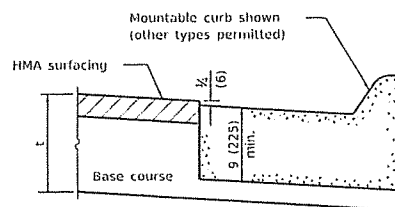
STANDARD 606001-07



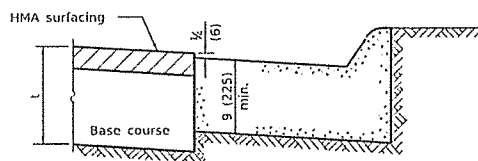
Unfolded contraction joint (typ.) construction options:

1. Form with $\frac{1}{2}$ (3) thick steel template 2 (50) deep, and seal.
2. Saw 2 (50) deep at 4 to 24 hours, and seal.
3. Insert $\frac{3}{4}$ (20) thick preformed joint filler full depth and width.

PLAN

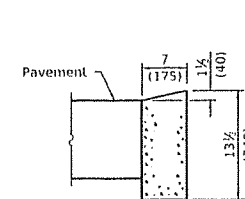


ON DISTURBED SUBGRADE

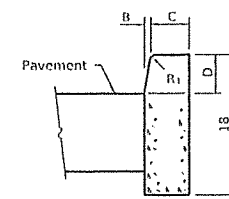


ON UNDISTURBED SUBGRADE

ADJACENT TO FLEXIBLE PAVEMENT

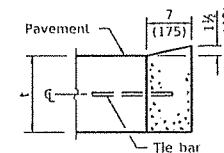


DEPRESSED CURB

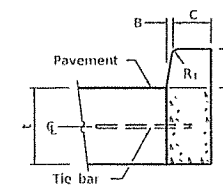


BARRIER CURB

ADJACENT TO FLEXIBLE PAVEMENT



DEPRESSED CURB



BARRIER CURB

ADJACENT TO PCC PAVEMENT OR PCC BASE COURSE

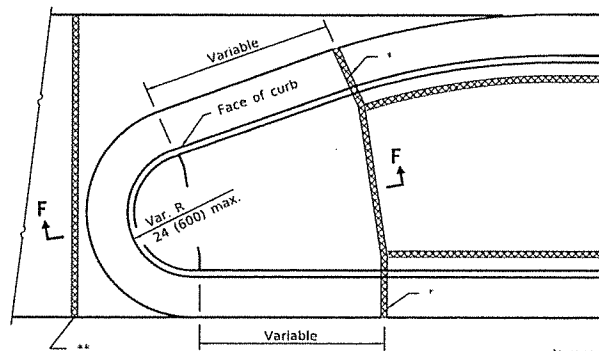
CONCRETE CURB TYPE B

CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER

(Sheet 2 of 2)

STANDARD 606001-07

Illinois Department of Transportation	
PASSED	January 1, 2018
[Signature]	
ENGINEER OF PUBLIC WORKS	
APPROVED	January 1, 2018
[Signature]	
LEGISLATIVE COUNSEL	

**TYPE P MEDIAN SURFACE**

¼ (20) PEIF between rigid pavement and median end. Align with joint in adjacent pavement.

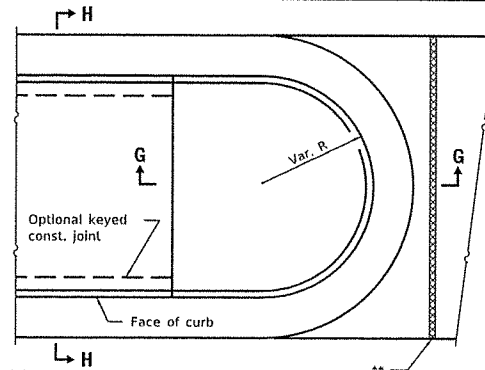
**SOLID MEDIAN**

TABLE OF DIMENSIONS						
TYPE SB MEDIAN						
TYPE	A	B	C	D	R ₁	
SB-6.06	6	1	6	6	1	
(SB-15.15)	(150)	(25)	(150)	(150)	(25)	
SB-6.12	12	1	6	6	1	
(SB-15.30)	(300)	(25)	(150)	(150)	(25)	
SB-6.18	18	1	6	6	1	
(SB-15.45)	(450)	(25)	(150)	(150)	(25)	
SB-6.24	24	1	6	6	1	
(SB-15.60)	(600)	(25)	(150)	(150)	(25)	
SB-9.06	6	2	5	9	1	
(SB-22.30)	(150)	(50)	(125)	(225)	(25)	
SB-9.12	12	2	5	9	1	
(SB-22.30)	(300)	(50)	(125)	(225)	(25)	
SB-9.18	18	2	5	9	1	
(SB-22.45)	(450)	(50)	(125)	(225)	(25)	
SB-9.24	24	2	5	9	1	
(SB-22.60)	(600)	(50)	(125)	(225)	(25)	

TABLE OF DIMENSIONS						
TYPE M AND SM MEDIAN						
TYPE	A	B	C	D	R ₁	
M-2.06	6	2	4	2	2	
(M-5.15)	(150)	(50)	(100)	(50)	(50)	
M-2.12	12	2	4	2	2	
(M-5.30)	(300)	(50)	(100)	(50)	(50)	
SM-4.06	6	4	3	4	3	
(SM-10.15)	(150)	(100)	(75)	(100)	(75)	
SM-4.12	12	4	3	4	3	
(SM-10.30)	(300)	(100)	(75)	(100)	(75)	
SM-4.18	18	4	3	4	3	
(SM-10.45)	(450)	(100)	(75)	(100)	(75)	
SM-4.24	24	4	3	4	3	
(SM-10.60)	(600)	(100)	(75)	(100)	(75)	
SM-6.06	6	6	2	6	2	
(SM-15.15)	(150)	(150)	(50)	(150)	(50)	
SM-6.12	12	6	2	6	2	
(SM-15.30)	(300)	(150)	(50)	(150)	(50)	
SM-6.18	18	6	2	6	2	
(SM-15.45)	(450)	(150)	(50)	(150)	(50)	
SM-6.24	24	6	2	6	2	
(SM-15.60)	(600)	(150)	(50)	(150)	(50)	

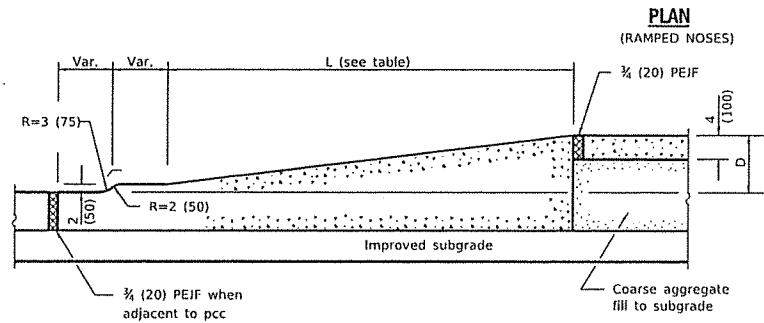
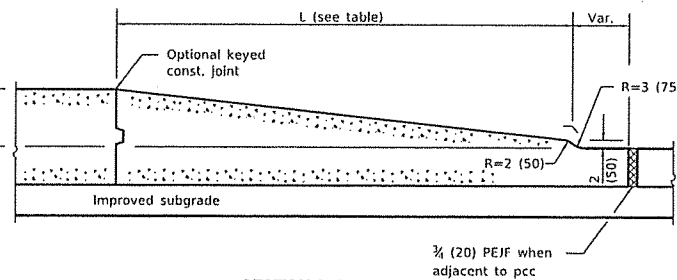
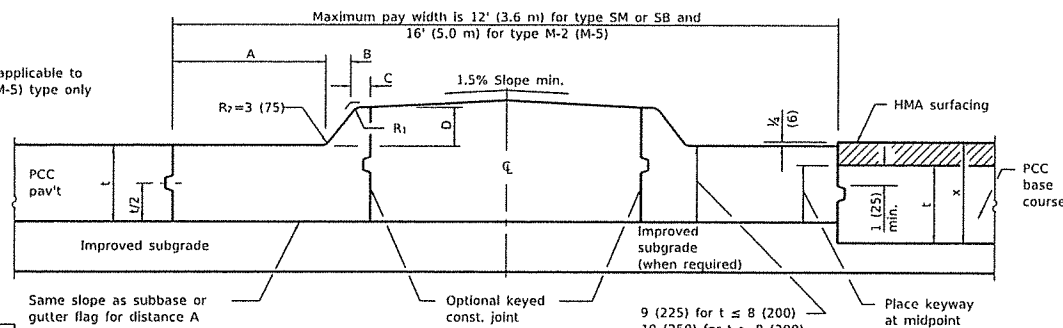
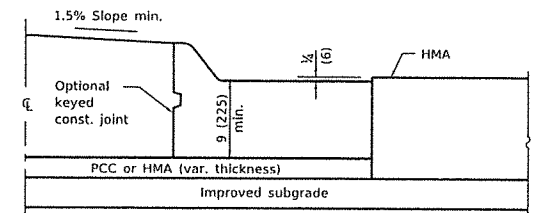
**SECTION F-F****SECTION G-G**

TABLE OF RAMPED NOSE LENGTHS	
TYPE OF NOSE	L
Median	6' (1.8 m)
Small Island	24 (600)
Intermediate Island	4' (1.2 m)
Large Island	6' (1.8 m)

NOTE
R₂ is applicable to
M-2 (M-5) type only

**HALF SECTION FOR PCC PAVEMENT****HALF SECTION FOR PCC BASE COURSE****SECTION H-H**

(TYPE SM, SB & M-5 (M-2) MEDIAN)

**HALF SECTION FOR FLEXIBLE PAVEMENT**

Illinois Department of Transportation

PASSED [] 2009

ENGINEER OF POLICY AND PROCEDURES

APPROVED [] 2009

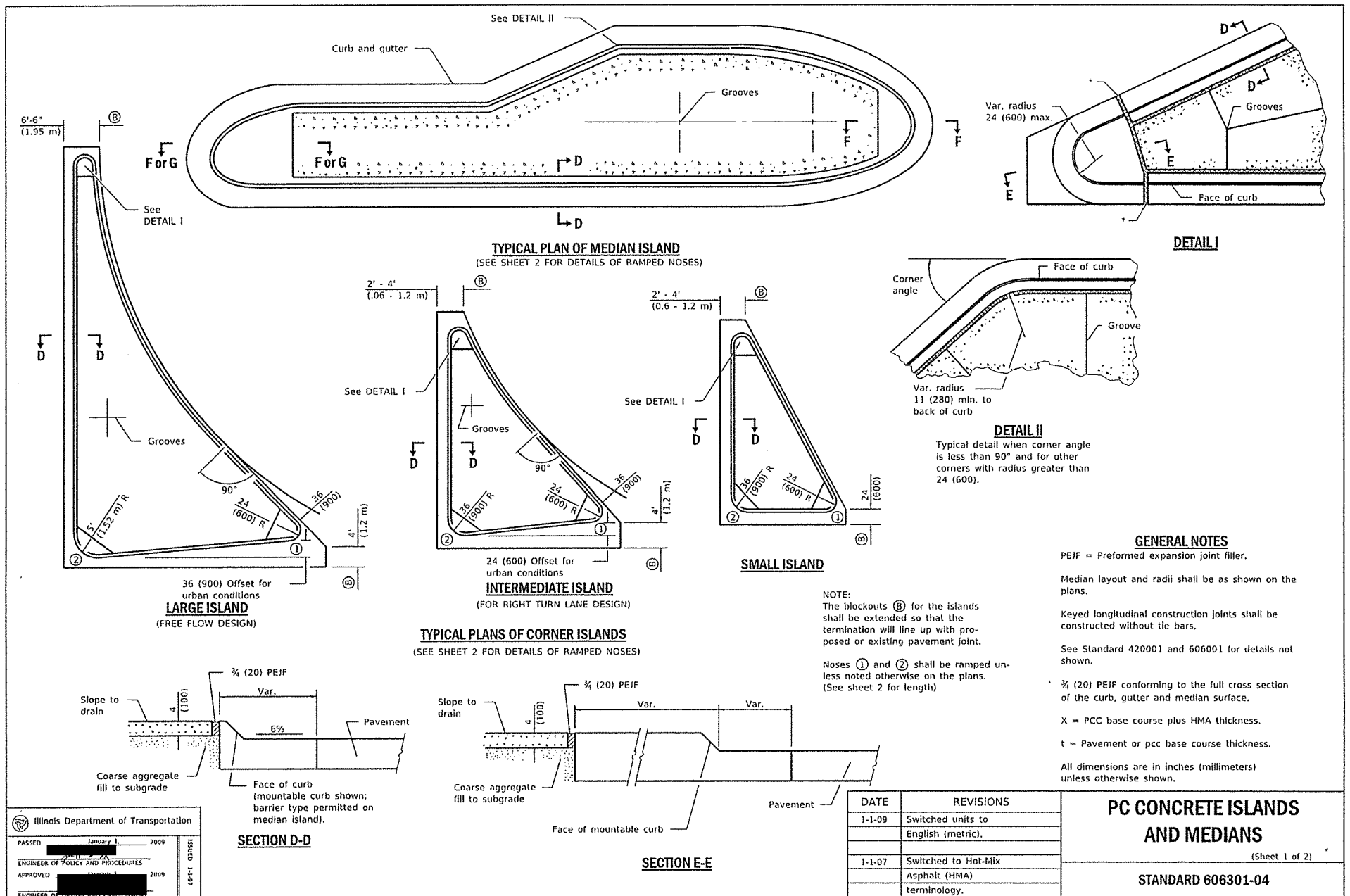
ENGINEER OF DESIGN AND ENVIRONMENT

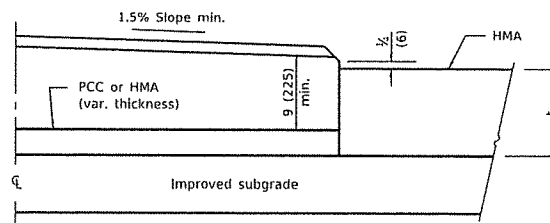
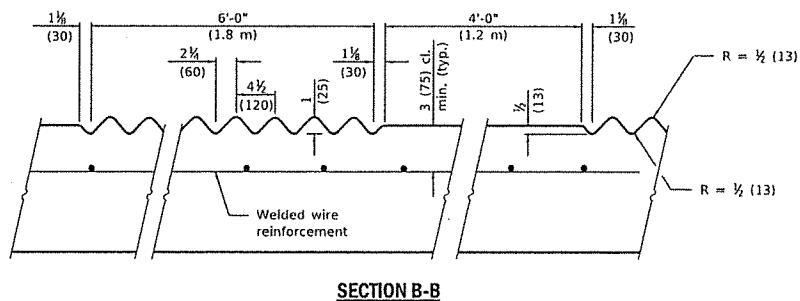
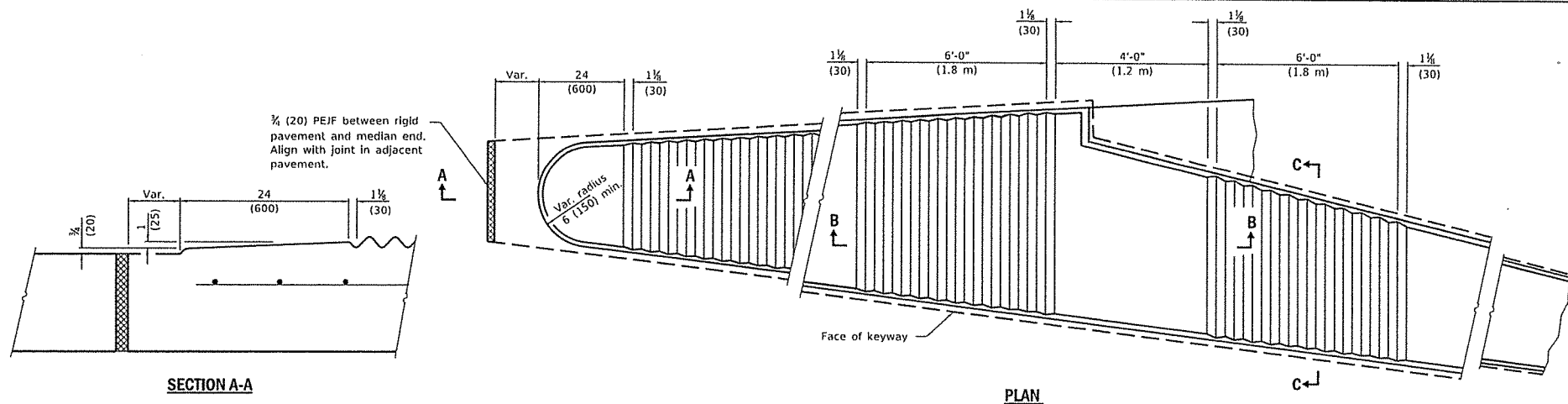
ISSUED 11-97

**PC CONCRETE ISLANDS
AND MEDIAN**

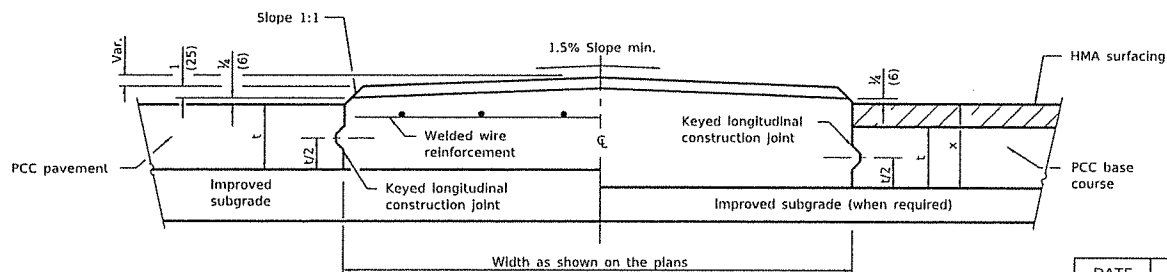
(Sheet 2 of 2)

STANDARD 606301-04





HALF SECTION FOR FLEXIBLE PAVEMENT
SECTION C-C



HALF SECTION FOR
PCC PAVEMENT

SECTION C-C

HALF SECTION FOR
PCC BASE COURSE

GENERAL NOTES

PEIF = Preformed expansion joint filler.

Median layout and radii shall be as shown on the plans.

Keyed longitudinal construction joints shall be constructed without tie bars.

X = PCC base course plus HMA thickness.

t = Pavement or pcc base course thickness.

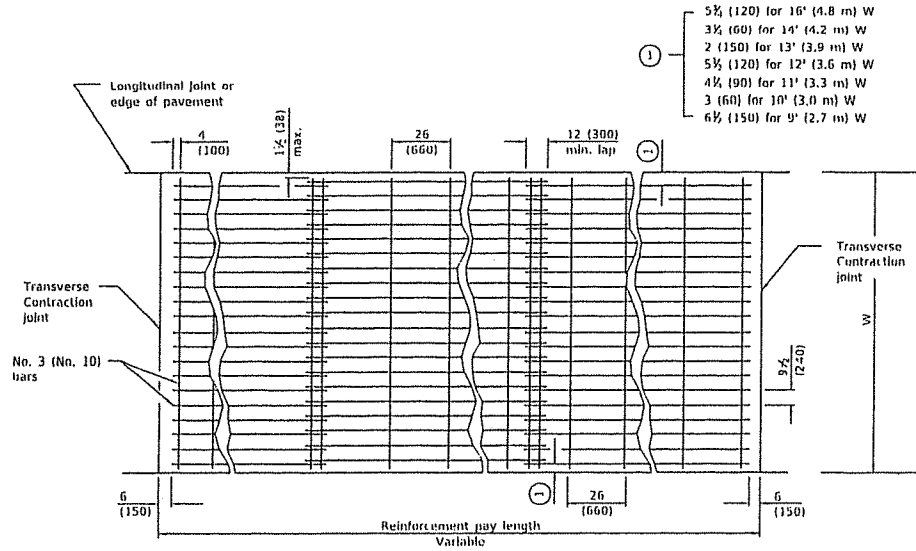
Welded wire reinforcement required for medians built contiguous to reinforced pcc pavement only.

See Standards 420001 and 420701 for details not shown.

All dimensions are in inches (millimeters) unless otherwise shown.

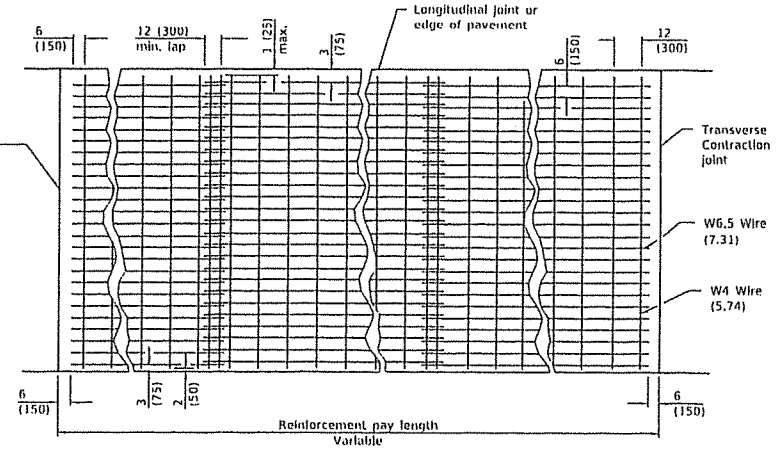
DATE	REVISIONS	CORRUGATED PC CONCRETE MEDIANS
4-1-16	Changed terminology to 'welded wire reinforcement'.	
1-1-09	Switched units to English (metric).	STANDARD 606306-04

Illinois Department of Transportation	
PASSED	2016
ENGINEER OF POLICY AND PROCEDURES	
APPROVED	2016
ENGINEER OF DESIGN AND ENVIRONMENT	



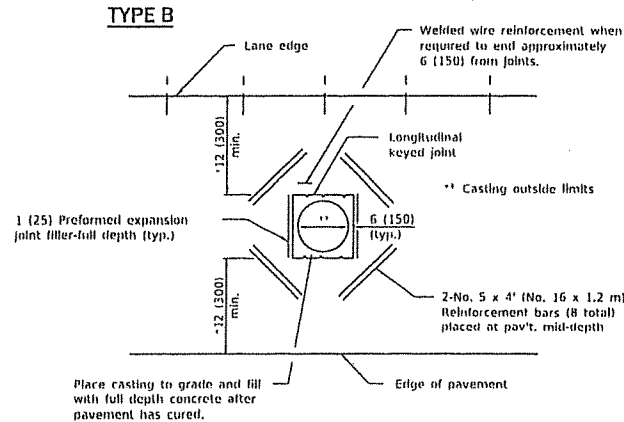
Approximately 63 lbs./100 sq. ft. (3.07 kg/m²)

When clipped bar mats are used, each bar intersection shall be clipped with W1.7 (3.74) wire.



Approximately 63 lbs./100 sq. ft. (3.07 kg/m²)

- When the 12 (300) minimum cannot be achieved, the transverse joints shall be extended to either the longitudinal joint or edge of pavement.



DETAIL OF ADDED REINFORCEMENT FOR PAVEMENT BLOCK-OUTS

TYPE A

GENERAL NOTES

Pavement block-outs shall be at least 24 (600) from contraction joints.

Welded wire reinforcement which is lapped longitudinally shall have a minimum lap of 6 (150).

Welded wire reinforcement may be positioned with the transverse wires on top or bottom of the longitudinal wires.

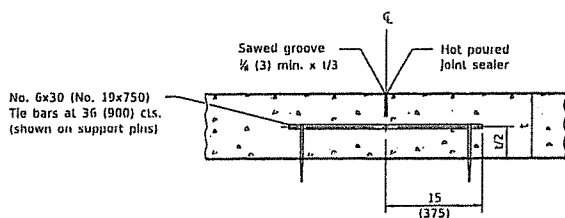
All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
4-1-16	Changed terminology to 'welded wire reinforcement'.
	Renamed standard.
1-1-08	Switched units to English (metric).

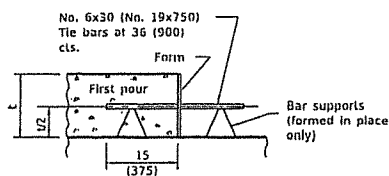
PAVEMENT WELDED WIRE REINFORCEMENT

STANDARD 420701-03

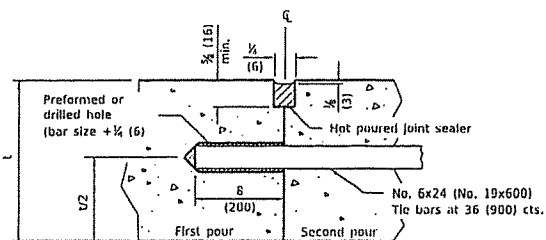
Illinois Department of Transportation	
DESIGNED BY	2016
ENGINEERED BY	2016
APPROVED BY	2016
ENGINEER OF DESIGN AND CONSTRUCTION	



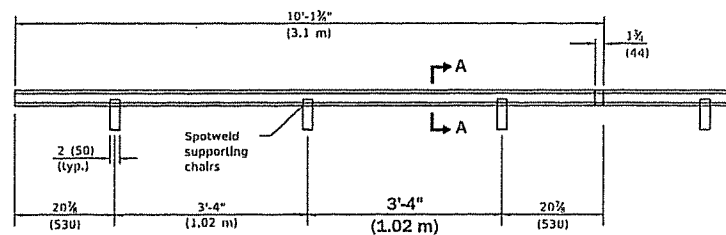
LONGITUDINAL SAWED JOINT



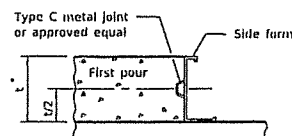
LONGITUDINAL
CONSTRUCTION JOINT
(TIE BAR FORMED IN PLACE
OR MECHANICALLY INSERTED)



LONGITUDINAL CONSTRUCTION JOINT
(TIE BAR GROUTED IN PLACE)

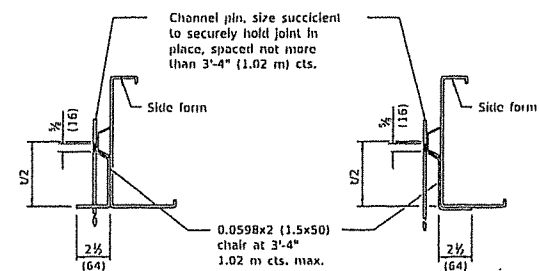


TYPE C METAL JOINT

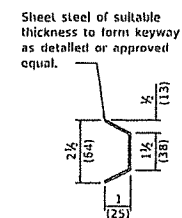


LONGITUDINAL KEYED JOINT

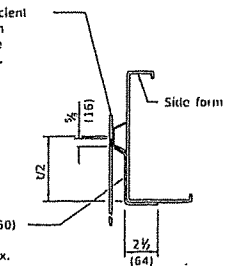
* 8 (203) mm. pavement thickness for keyed joints.



SUPPORTING CHAIR
ALTERNATE



SECTION A-A



SUPPORTING CHAIR
ALTERNATE

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

PASSED January 1, 2010

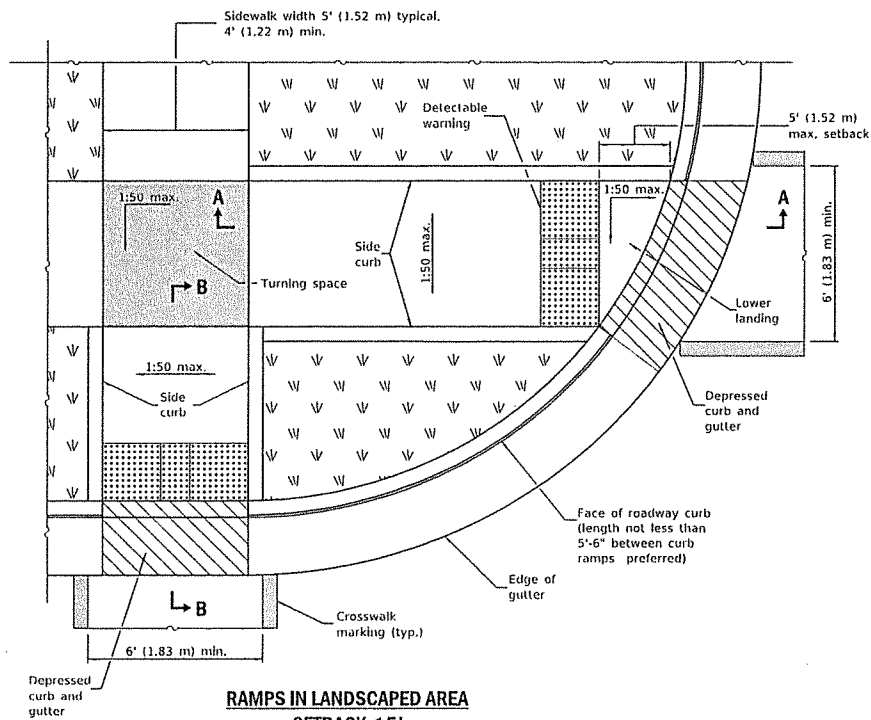
ISSUED 1-1-07

ENGINEER OF POLICY AND PROCEDURES

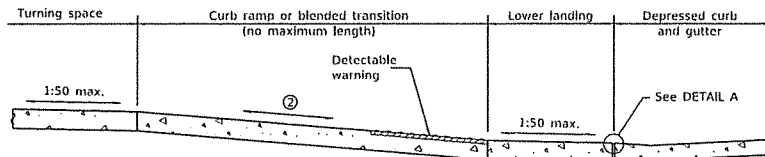
APPROVED January 1, 2010

EXHIBIT

DATE	REVISIONS	<div>PAVEMENT JOINTS</div> <div>(Sheet 1 of 2)</div> <div>STANDARD 420001-09</div>
1-1-18	Changed tie bar spacing to 36 (900) cts. Revised DOWEL BAR TABLE.	
1-1-08	Switched units to English (metric).	

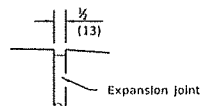


RAMPS IN LANDSCAPED AREA
SETBACK $\leq 5'$

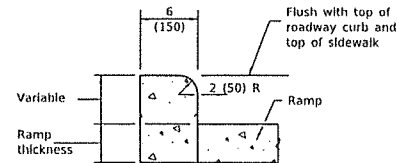


SECTION A-A

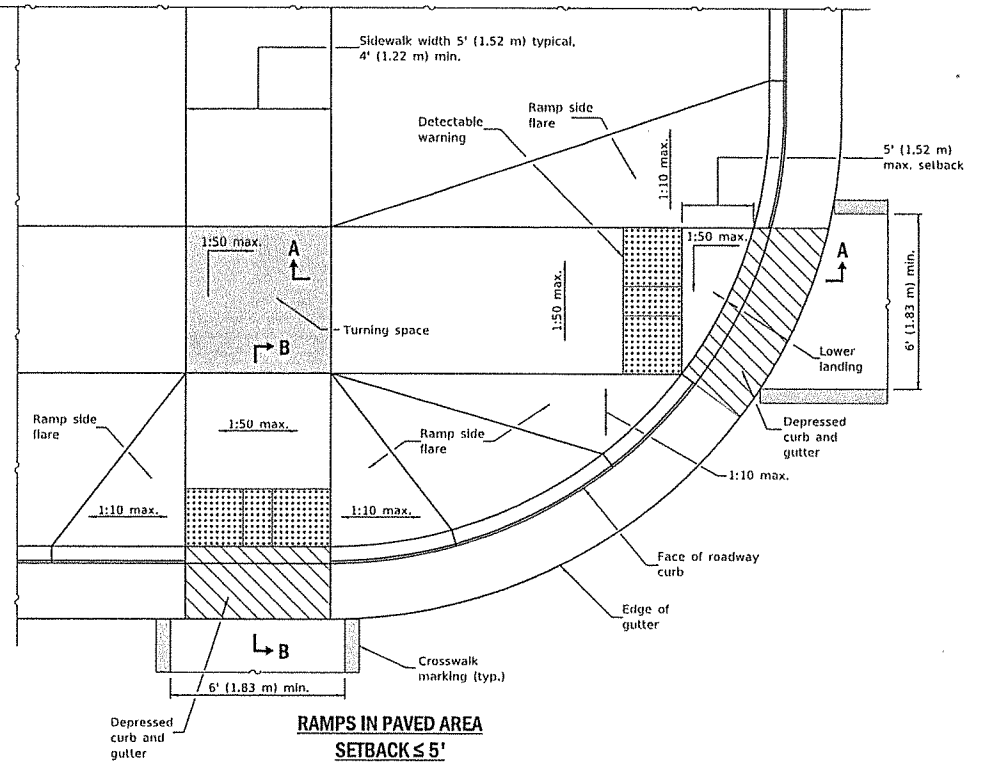
- ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



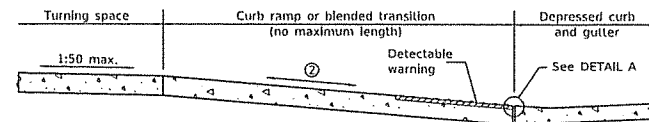
DETAIL A



SIDE CURB DETAIL



RAMPS IN PAVED AREA
SETBACK $\leq 5'$



SECTION B-B

- ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

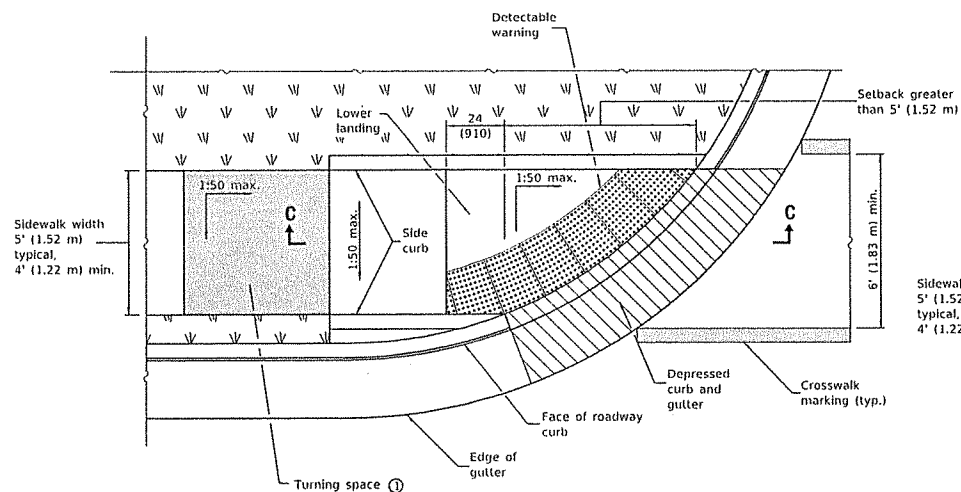
See Sheet 2 for GENERAL NOTES.

DATE	REVISIONS
1-1-19	Removed "15-foot rule", added "Blended transitions" and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at turning spaces and lower landings.

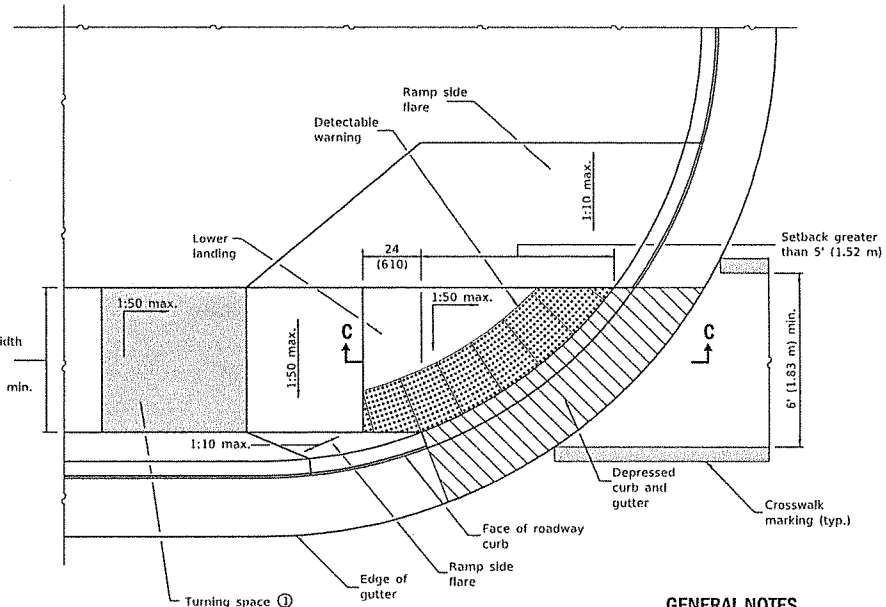
PERPENDICULAR CURB RAMPS FOR SIDEWALKS

(Sheet 1 of 2)

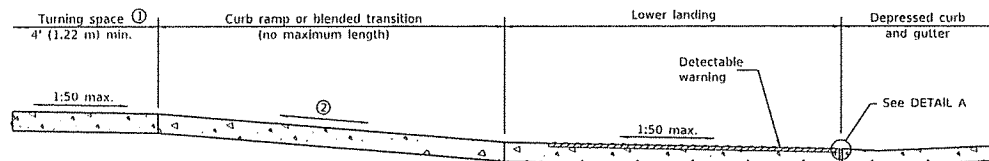
STANDARD 424001-11



RAMP IN LANDSCAPED AREA
SETBACK > 5'



RAMP IN PAVED AREA
SETBACK > 5'



SECTION C-C

- ① This turning space not required for blended transitions.
- ② The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

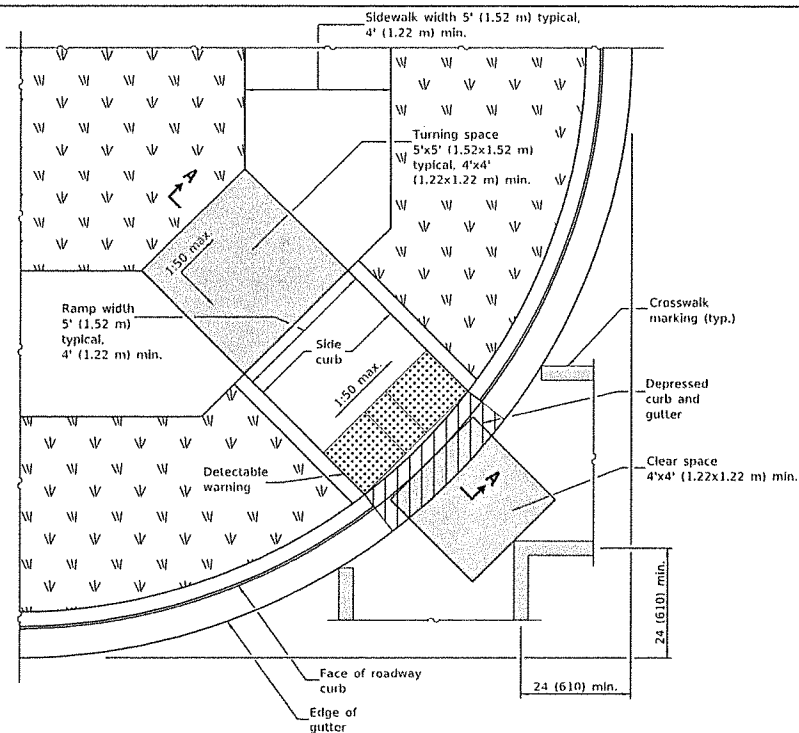
All dimensions are in inches (millimeters) unless otherwise shown.

**PERPENDICULAR CURB RAMPS
FOR SIDEWALKS**

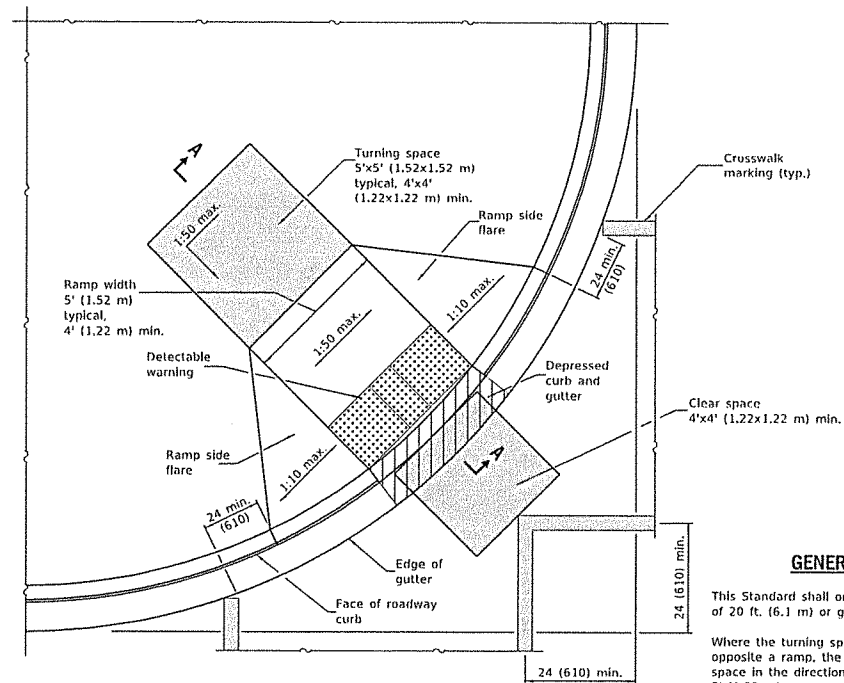
(Sheet 2 of 2)

STANDARD 424001-11

Illinois Department of Transportation	
DESIGNED BY	2019
ENGINEER OF POLICY AND PROCEDURES	
APPROVED BY	2019
ENGINEER	DATE



RAMP IN LANDSCAPED AREA



RAMP IN PAVED AREA

GENERAL NOTES

This Standard shall only be used for curb radii of 20 ft. (6.1 m) or greater.

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

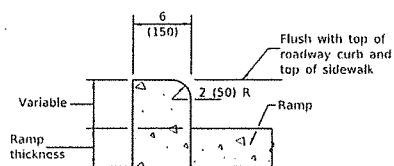
Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

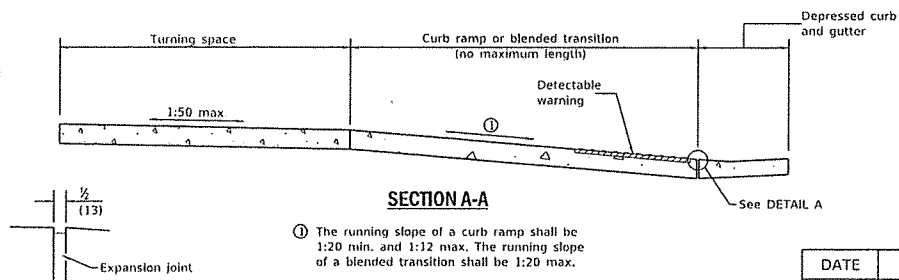
All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

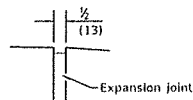


SIDE CURB DETAIL



SECTION A-A

① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

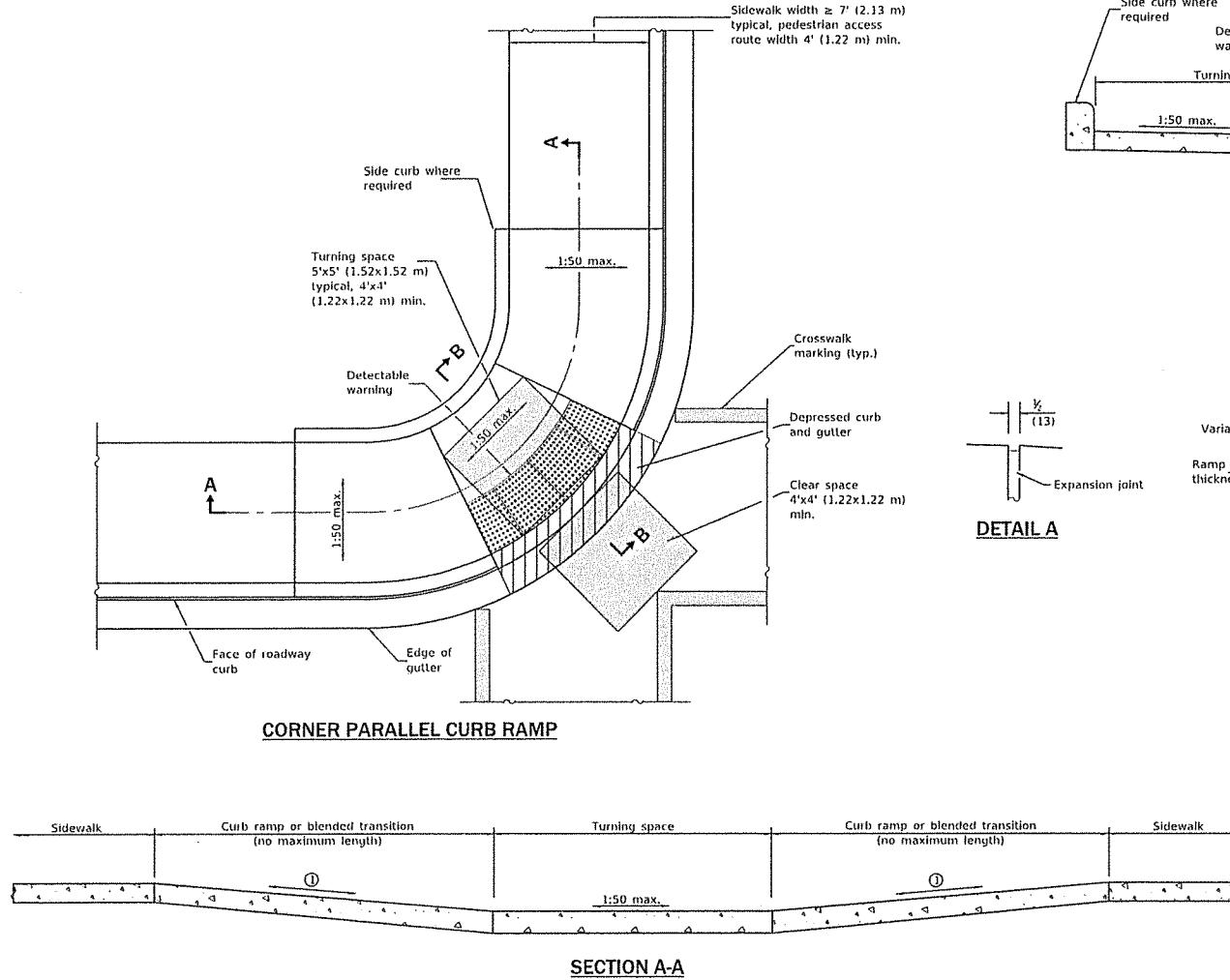


DETAIL A

DATE	REVISIONS
1-1-19	Removed "15-foot rule", added "blended transitions" and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at turning spaces.

DIAGONAL CURB RAMPS FOR SIDEWALKS

STANDARD 424006-04



① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

SECTION B-B

DETAIL A

SIDE CURB DETAIL

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Removed upper landing, added blended transition and detectable warning tolerances.
1-1-17	Revised sidewalk width to include 24 (610) buffer behind curb.

CORNER PARALLEL CURB RAMPS FOR SIDEWALKS

STANDARD 424011-04

Illinois Department of Transportation

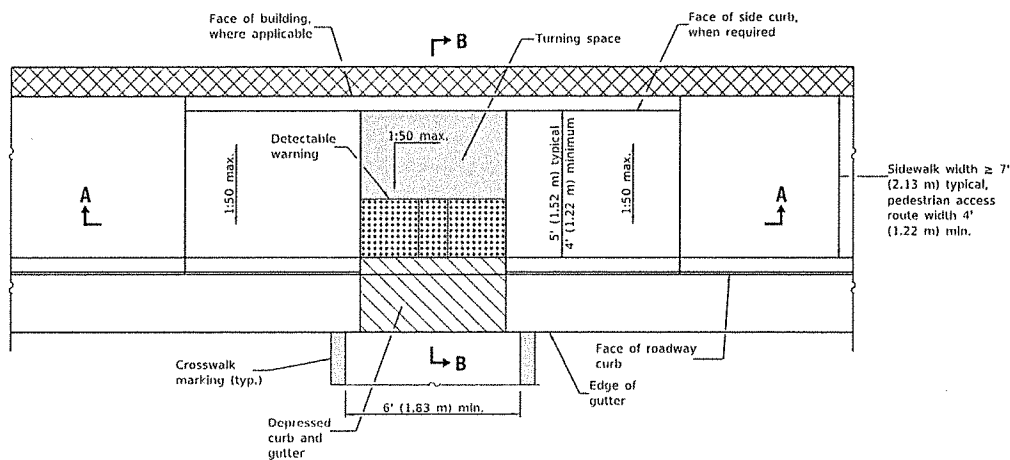
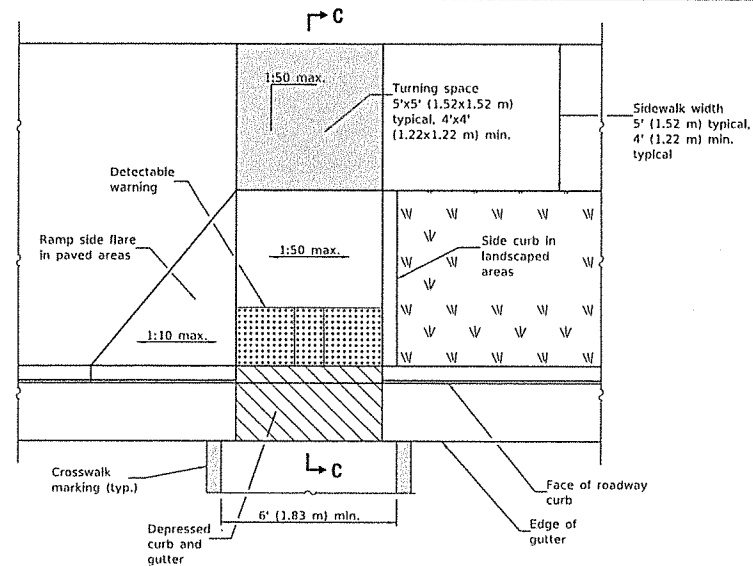
PAS: [REDACTED] 2019

ENGINEER OF POLICY AND PROCEDURES

APPROVED: [REDACTED] 2019

ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12

**PARALLEL MID-BLOCK CURB RAMP****PERPENDICULAR MID-BLOCK CURB RAMP****GENERAL NOTES**

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

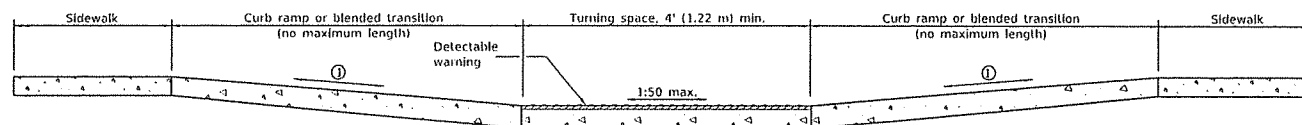
Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in. width is allowed.

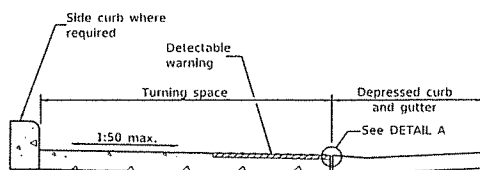
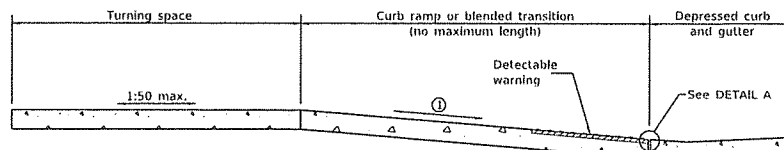
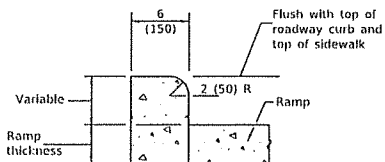
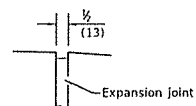
Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

**SECTION A-A**

- ① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.

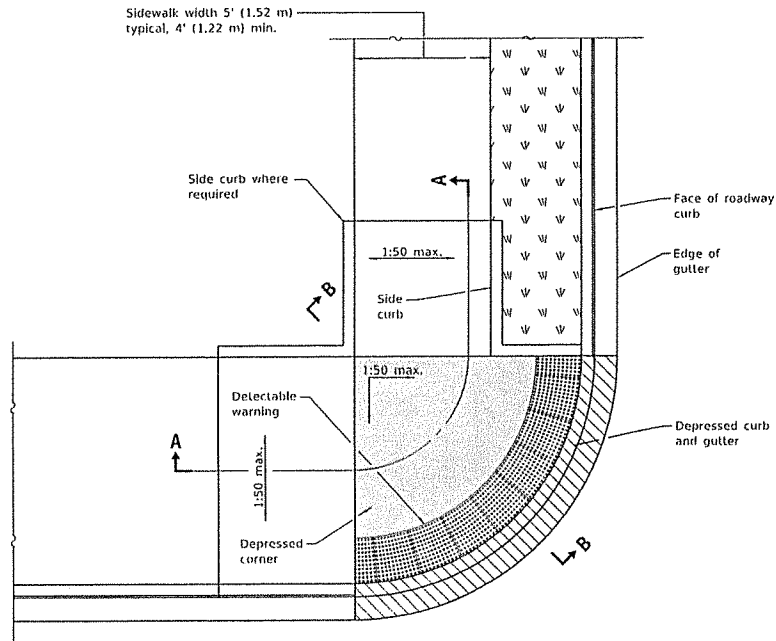
**SECTION B-B****SECTION C-C****SIDE CURB DETAIL****DETAIL A**

DATE	REVISIONS
1-1-19	Removed upper landing, added blended transitions and detectable warning tolerances.
1-1-18	Omitted diagonal slope at landings.

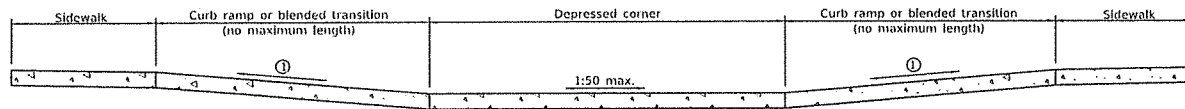
MID-BLOCK CURB RAMPS FOR SIDEWALKS

STANDARD 424016-05

Illinois Department of Transportation	
PASS	July 1, 2019
ENGINEER OF POLICY AND PROCEDURES	
APPROVED	September 1, 2019
22-1-12	

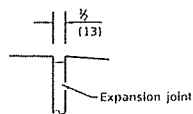


DEPRESSED CORNER

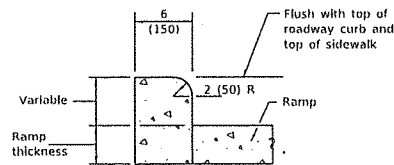


SECTION A-A

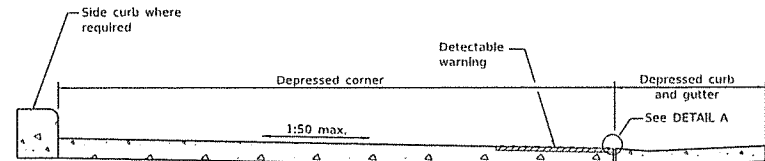
① The running slope of a curb ramp shall be 1:20 min. and 1:12 max. The running slope of a blended transition shall be 1:20 max.



DETAIL A



SIDE CURB DETAIL



SECTION B-B

GENERAL NOTES

This standard shall only be used for curb radii of 6 ft. (1.83 m) or greater.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal tolerances but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in. width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-19	Removed upper landings, added blended transition and detectable warning tolerances.
1-1-18	Omitted diagonal slope at turning spaces and upper landings.

**DEPRESSED CORNER
FOR SIDEWALKS**

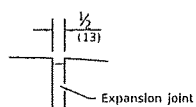
STANDARD 424021-05

Illinois Department of Transportation	
PASS	2019
ENGINEER OF POLICY AND PROCEDURES	
APP	2019
ENG	2019

The diagram illustrates a curb ramp or blended transition with the following labeled components and dimensions:

- Sidewalk**: The flat surface on the left.
- Turning space, 4' (1.22 m) min.**: A dimensioned area on the sidewalk.
- Curb ramp or blended transition (no maximum length)**: The sloped section connecting the sidewalk to the entrance.
- Entrance or alley**: The destination area on the right.
- 1:50 max.**: A dimension indicating the maximum slope of the ramp.
- Detectable warning**: A textured surface at the top of the ramp, labeled with a circled 2.
- See DETAIL A**: A callout pointing to a circular detail at the bottom right corner of the ramp.
- ④**: A circled number 4 with an arrow pointing left, indicating a specific surface or material.

- ④ The running slope of a curb ramp shall be 1:20 min and 1:12 max. The running slope of a blended transition shall be 1:20 max.



The diagram illustrates a pedestrian crossing layout with three distinct sections. The first section on the left is labeled 'Entrance or alley' and has a width indicated as 'Varies'. The middle section is labeled 'Pedestrian crossing' and has a width indicated as '1.50 max.'. The third section on the right is labeled 'Entrance or alley return' and also has a width indicated as 'Varies'. The crossing section is shown with a pattern of dots, suggesting a textured surface or a specific material.

Figure 1 is a cross-sectional diagram of a concrete curb and sidewalk. The curb is a rectangular block labeled '6 (150)' with a 'Variable' thickness. The sidewalk is a thinner layer labeled '2 (50) R' with a 'Ramp thickness' indicated. The curb is 'Flush with top of roadway curb and top of sidewalk'. The sidewalk is shown with a cross-hatched pattern. The curb has a 'Ramp' on its side.

DATE	REVISIONS
1-1-19	Added blended transitions and placement tolerances for detectable warnings.
1-1-18	Omitted diagonal slope at upper landings.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

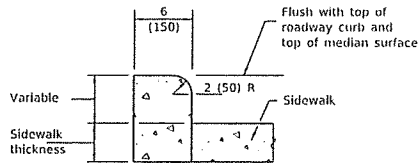
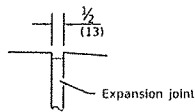
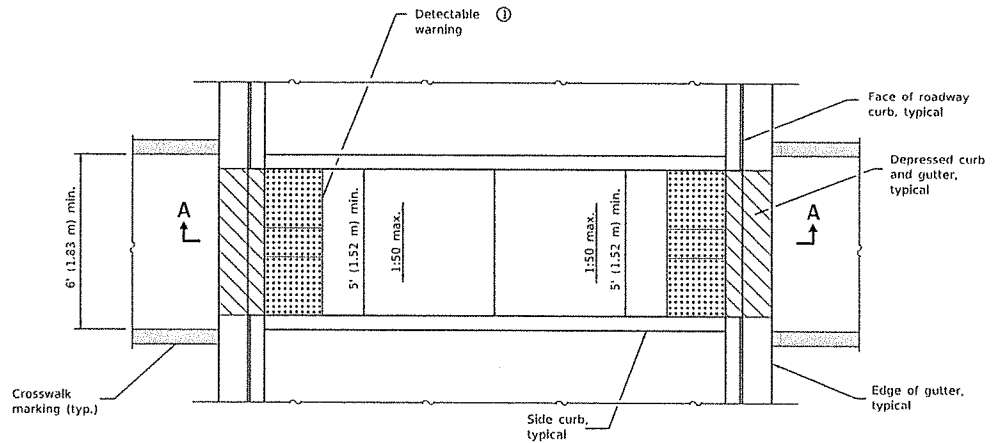
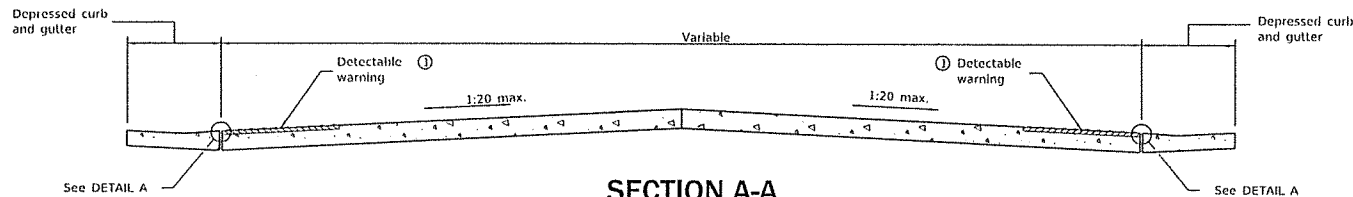
Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

All dimensions are in inches (millimeters) unless otherwise shown.

**ENTRANCE / ALLEY
PEDESTRIAN CROSSINGS**

STANDARD 424026-03

**SIDE CURB DETAIL****DETAIL A****MEDIAN PEDESTRIAN CROSSING****SECTION A-A**

① Omit detectable warnings when distance between back of curbs is less than 6' (1.83 m).

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

Detectable warnings are shown in their ideal locations but the following placement tolerances are allowed.

Side Border - Detectable warnings should extend the full width of the walking surface (excluding flared sides) but a border along each side up to 2 in. (50 mm) in width is allowed.

Curb Set-Back - Detectable warnings located at the back of curb should closely align with the curb but a gap up to 6 in. (150 mm) behind the curb is allowed.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS	MEDIAN PEDESTRIAN CROSSINGS
1-1-19	Added placement tolerances for detectable warnings.	
1-1-12	Widened crosswalk to 6' (1.83 m) min. inside dimension.	STANDARD 424031-02
	Revised General Notes.	

Illinois Department of Transportation	
DESIGNED BY: [REDACTED] 2019	ISSUED: 1-1-12
ENGINEERED BY: [REDACTED] 2019	
APPROVED BY: [REDACTED] 2019	

SPECIAL PROVISIONS
for
COMMERCIAL ENTRANCE

PIPE CULVERTS - Pipe culverts shall be either new reinforced concrete culvert pipe or corrugated metal culvert pipe. The size and length of the pipe culvert to be installed are shown on the permit plan sheet and shall not be revised by the Applicant without approval from the Department.

DRIVEWAY SURFACE - The driveway surface shall be paved to the right-of-way line or to a point designated on the plans with Portland Cement Concrete. Portland Cement Concrete driveways shall have a minimum thickness of eight (8) inches and be constructed with the Illinois Department of Transportation's **Class "PV" Concrete Mix**. All material shall be from a state approved source.

DRIVEWAY GRADES - All driveways constructed in rural locations shall have a grade that slopes downward and away from the highway surface at a rate equal to the slope of the shoulder, but not less than 3/16-inch nor greater than 1-inch per foot. This slope shall continue for a minimum distance of 10 feet or to the center line of the ditch, whichever is less. The initial slope within the shoulder area of all drives constructed in urban locations shall be compatible with the provisions for drainage of the existing cross section. The grade used shall accommodate the flow of the drainage in the vicinity of the driveway and should be designed so that future widening of the highway would not require major reconstruction of the driveway.

Beyond the shoulder, the grade of rural commercial driveways within the right-of-way should not exceed 10 percent. The slopes of all drives constructed in urban locations shall be compatible with the provisions for drainage of the designed cross section, but shall not exceed 6 percent for commercial driveways.

DRIVEWAY ENCROACHMENT - No portion of the driveway shall encroach on or extend in front of an adjacent property owner's frontage without first submitting to this Department written permission from said adjacent property owner for such encroachment.

USE OF DRAINAGE FACILITIES - In granting this permit, it is understood that the uses of drainage facilities of any nature are to comply with the requirements of the state laws, local ordinances and the regulations of the Illinois Environmental Protection Agency, Division of Water Pollution Control. No person, firm, corporation, or institution, public or private, shall discharge or empty any type of domestic, commercial or industrial waste, or putrescible liquids, or cause the same to be discharged or emptied in any manner into open ditches along any public street or highway, or into any drain or drainage structure installed solely for street or highway drainage purposes.

MAINTENANCE - It is understood and agreed that the Applicant, his successors and assigns shall construct and maintain that portion of the driveway on State right-of-way in a manner satisfactory to the Department.

TRAFFIC CONTROL - Whenever any of the work under this permit involves any obstruction or hazard to the free flow of traffic in the normal traffic lanes, plans for the proposed method of traffic control must be submitted to and approved by the District Engineer at least 72 hours, and preferably longer, before the start of work.

SPECIAL PROVISIONS FOR COMMERCIAL ENTRANCES (Continued)

All traffic control shall be in accordance with the State of Illinois Manual on Uniform Traffic Control Devices and amendments thereof. It should be noted that standard and typical placement of devices shown in the Uniform Manual are minimums. Many locations may require additional or supplemental devices.

The state of Illinois, through its representative, reserves the right to stop work on this project anytime it is determined that requirements of the permit are being violated.

SEEDING REQUIREMENTS – The Applicant will fertilize, seed and straw the slopes that are damaged. The quantities are as follows:

Seed:	<u>Spring</u>	<u>Fall</u>
Ky. Bluegrass	50 lbs.	50 lbs.
Perennial Ryegrass	20	20
Redtop or Creeping Red Fescue	10	10
Ladino or White Dutch Clover	5	0
Oats, Spring	0	48

Fertilizer: 10-6-4 800 lbs. per acre

Straw: 2 tons per acre

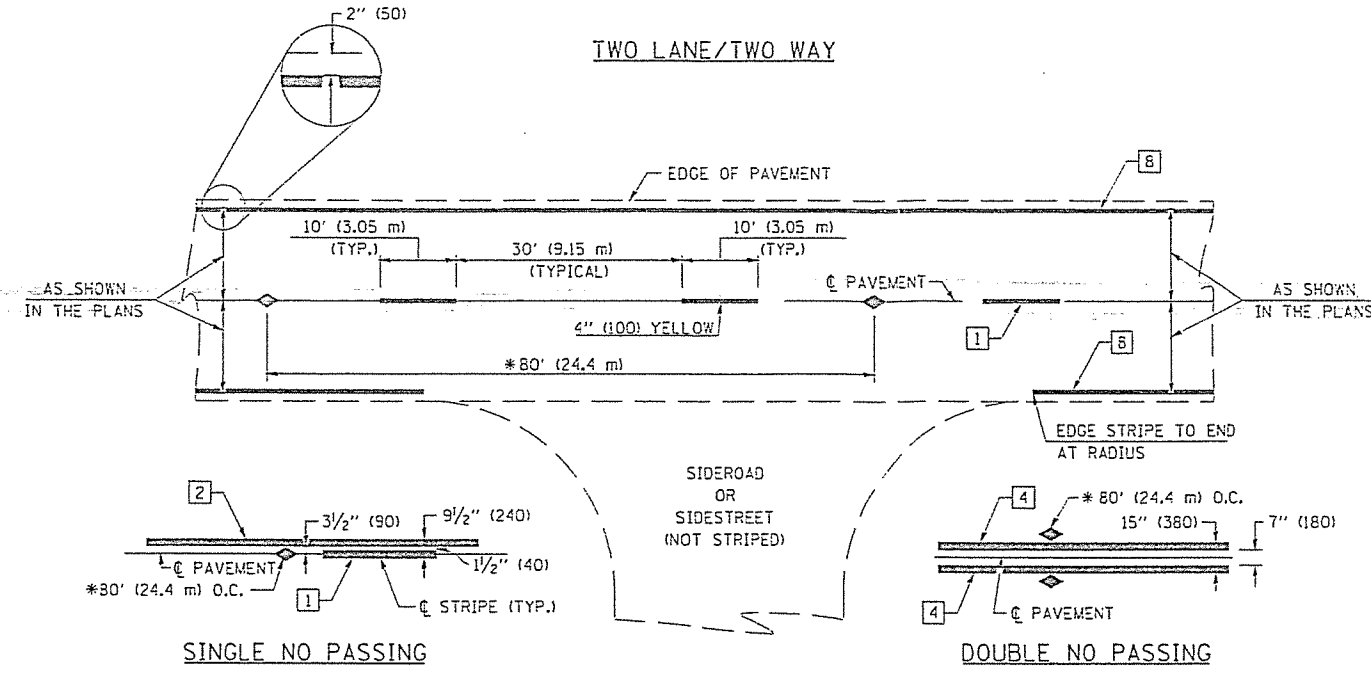
All waste materials and debris shall be removed from state highway right-of-way.

BUILDING, SET-BACK LINES OR ZONING – The Applicant, his successors and assigns agree to abide by the terms of all resolutions passed by LOCAL AUTHORITIES (Township, County and Municipality) in regard to the establishment of building set-back lines or zoning in the County. The proper Local Authority should be contacted for further information.

RIGHT-OF-WAY – The right-of-way dimensions shown on this permit are taken from our plans. However, it is the applicant's responsibility to establish the correct property lines.

BURIED UTILITIES – The Applicant shall contact the appropriate utility companies for the location of buried utilities before starting work on this permit.

ADVERTISING SIGNS – The Applicant, his successor and assigns, shall NOT place any advertising material of any kind on, upon, or over State Highway right-of-way.



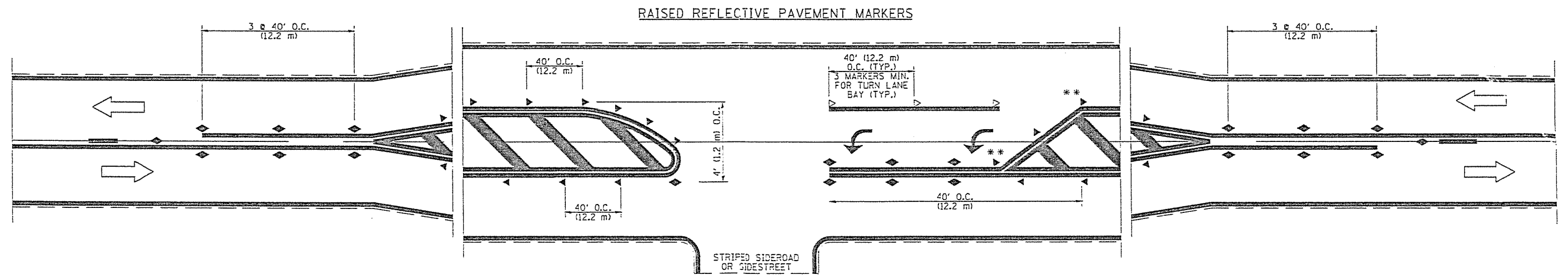
* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

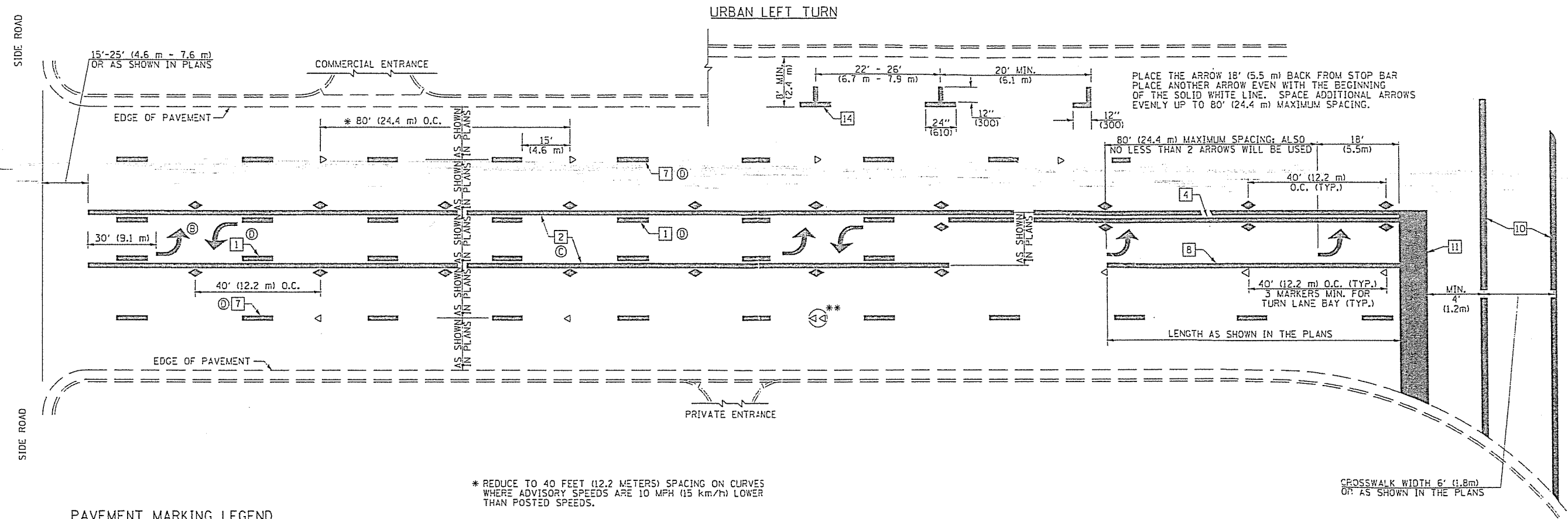


** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEET SHEETS	SHEET NO.
Plot Date = 3/6/2010	Plot Scale = 1/8" = 1' ON	DRAWN -	REVISED -							
		CHECKED -	REVISED -							
		DATE -	REVISED -							
SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.						CONTRACT NO.				
						ILLINOISIFIED, AID PROJECT				



PAVEMENT MARKING LEGEND

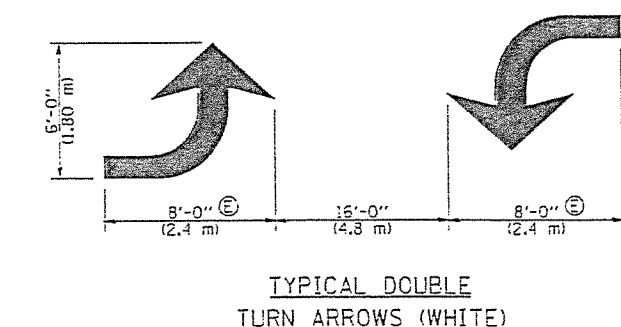
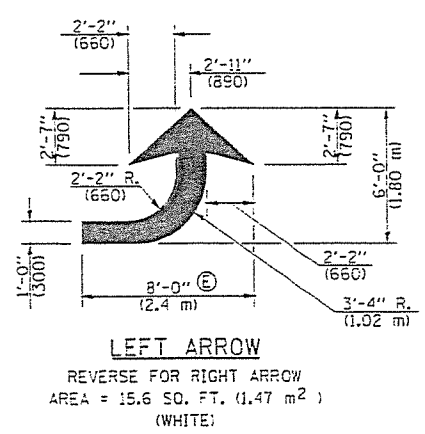
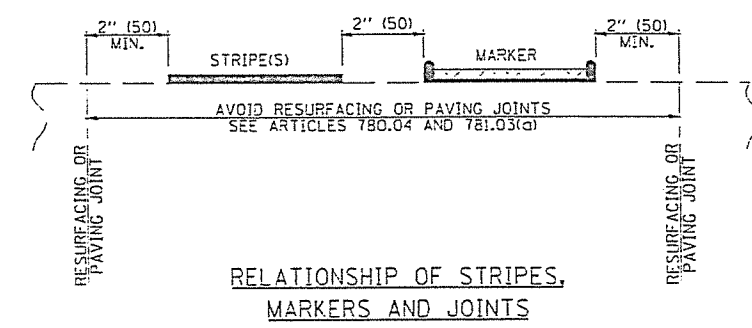
- | | | |
|----|---------------------------------------|--|
| 1 | 4" (100) SKIP-DASH (YELLOW) | |
| 2 | 4" (100) SOLID (YELLOW) | |
| 3 | 12" (300) DIAGONAL (YELLOW) | |
| 4 | 4" (100) DOUBLE YELLOW (NARROW) | |
| 5 | RESERVED | |
| 6 | RESERVED | |
| 7 | 6" (150) SKIP-DASH (WHITE) | |
| 8 | 4" (100) SOLID (WHITE) | |
| 9 | 12" (300) DIAGONAL (WHITE) | |
| 10 | 6" (150) SOLID (WHITE) | |
| 11 | 24" (600) STOP BAR (WHITE) | |
| 12 | 8" (200) SOLID (WHITE) | |
| 13 | 6" (100) LANE LINE EXTENSIONS (WHITE) | |
| 14 | 4" (100) PARKING WHITE | |

* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

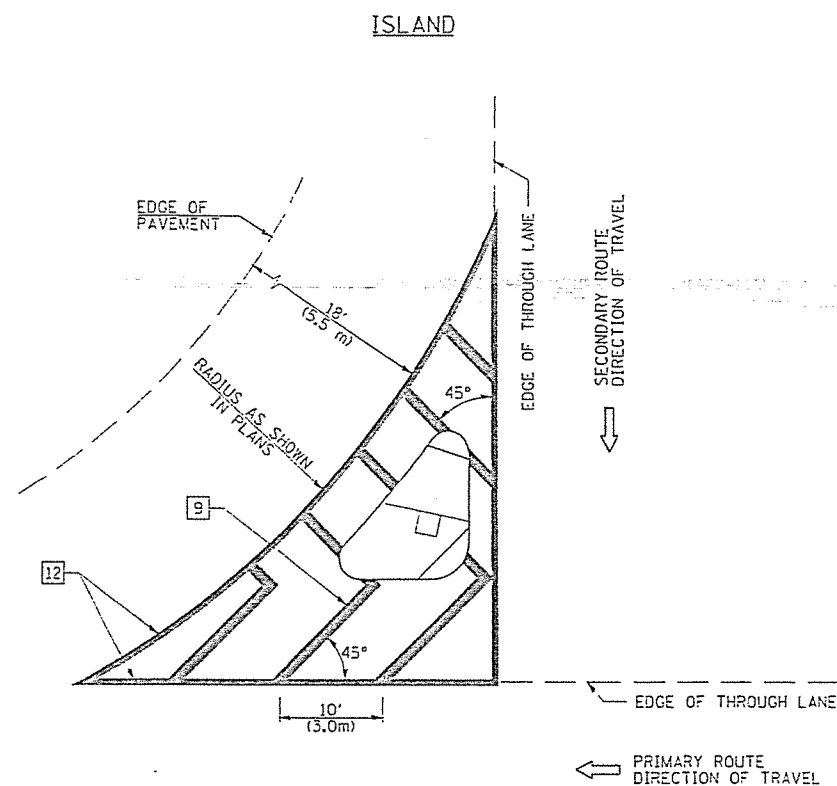
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

GENERAL NOTES

- B TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- C THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- D THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- E USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780X FOR SYMBOLS TABLE)



NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.



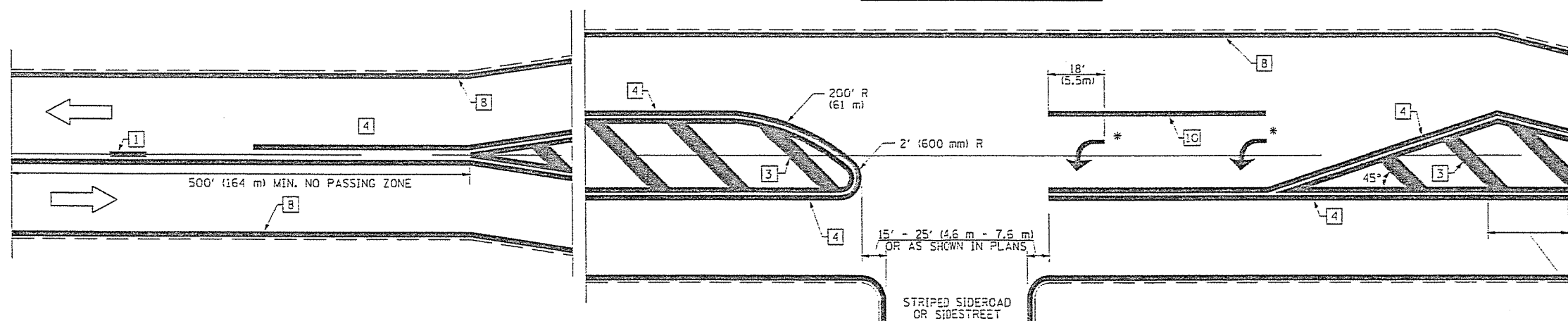
GENERAL NOTES

1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

<30 MPH (<50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
>45 MPH (>75 km/h)	30' (9.0 m)

- | | |
|------|-------------------------------|
| [7] | 6" |
| [8] | 4" (100) |
| [9] | 12" (300) DIA |
| [10] | 6" (150) SOLID (WH) |
| [11] | 24" (600) STOP BAR (WH) |
| [12] | 8" (200) SOLID (WHITE) |
| [13] | 6" (100) LANE LINE EXTENSIONS |
| [14] | 4" (100) PARKING WHITE |

RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

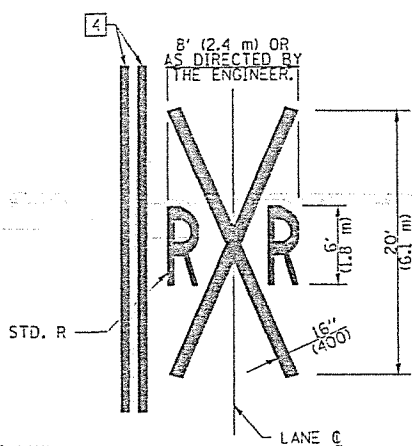
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PLOT SCALE = 3.0000" / IN.		CHECKED -	REVISED -
PLOT DATE = 5/8/2013		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT
(RURAL & URBAN APPLICATIONS)

SCALE: | SHEET NO. 3 OF 4 SHEETS | STA. | TC

PAVEMENT MARKING LEGEND

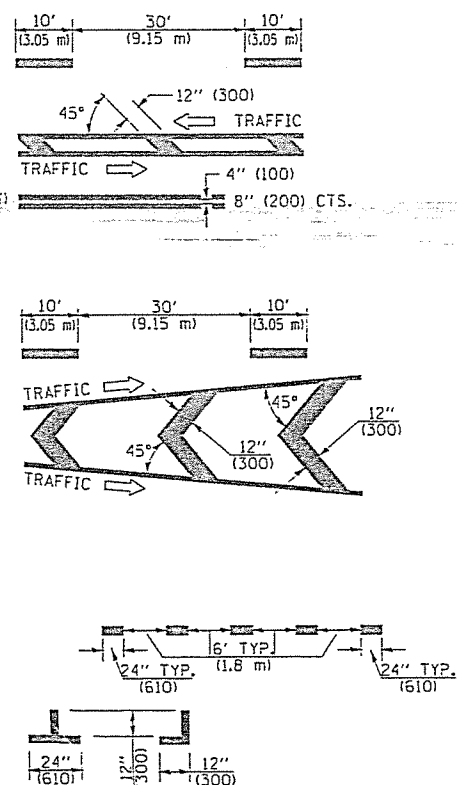


THE TRAVERSE SPREAD OF THE "X"
MAY VARY ACCORDING TO LANE WIDTH.

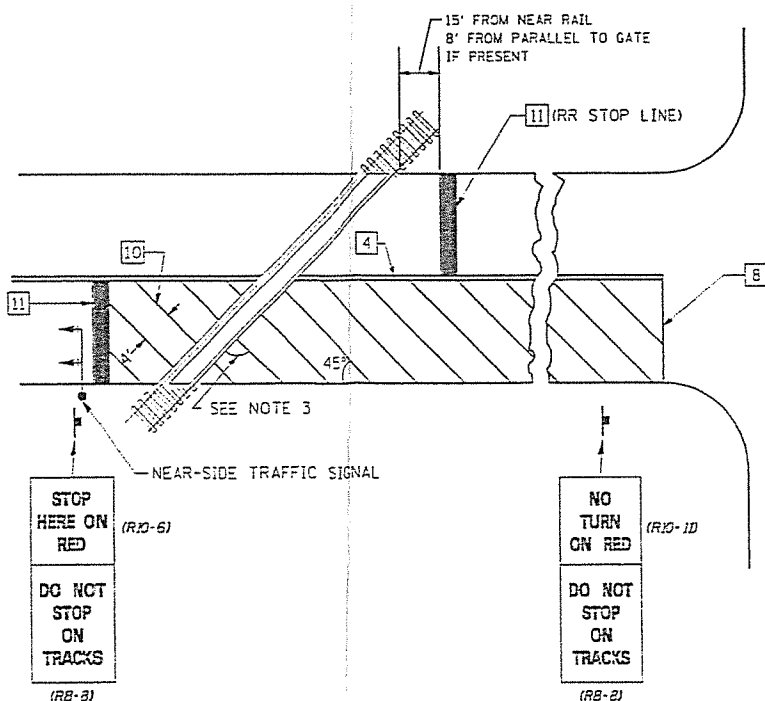
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE 11-1, CONDITION B OF THE MUTCD.

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS
- 14 4" (100) PARKING WHITE



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



1. SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
3. WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20° THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

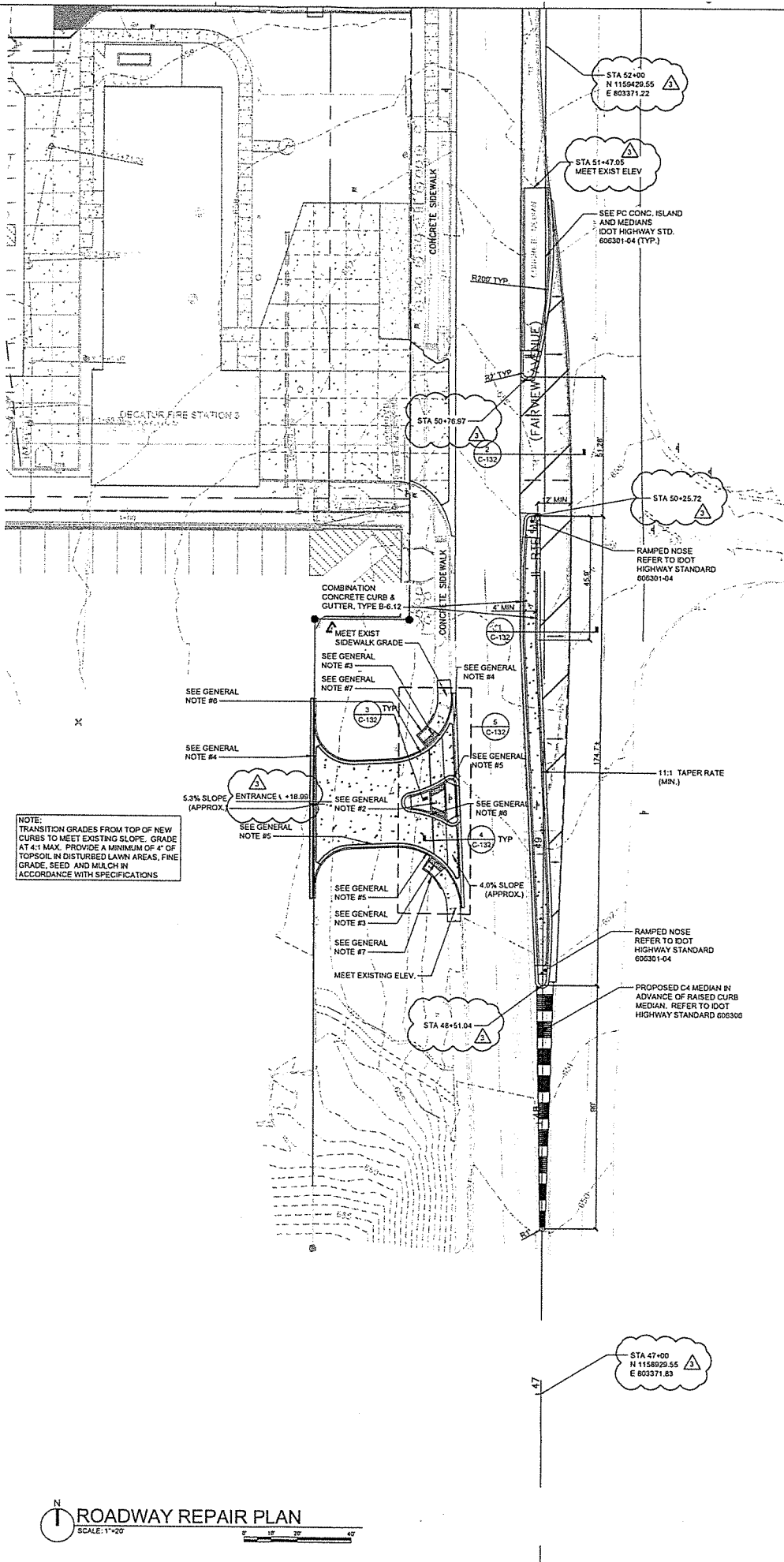
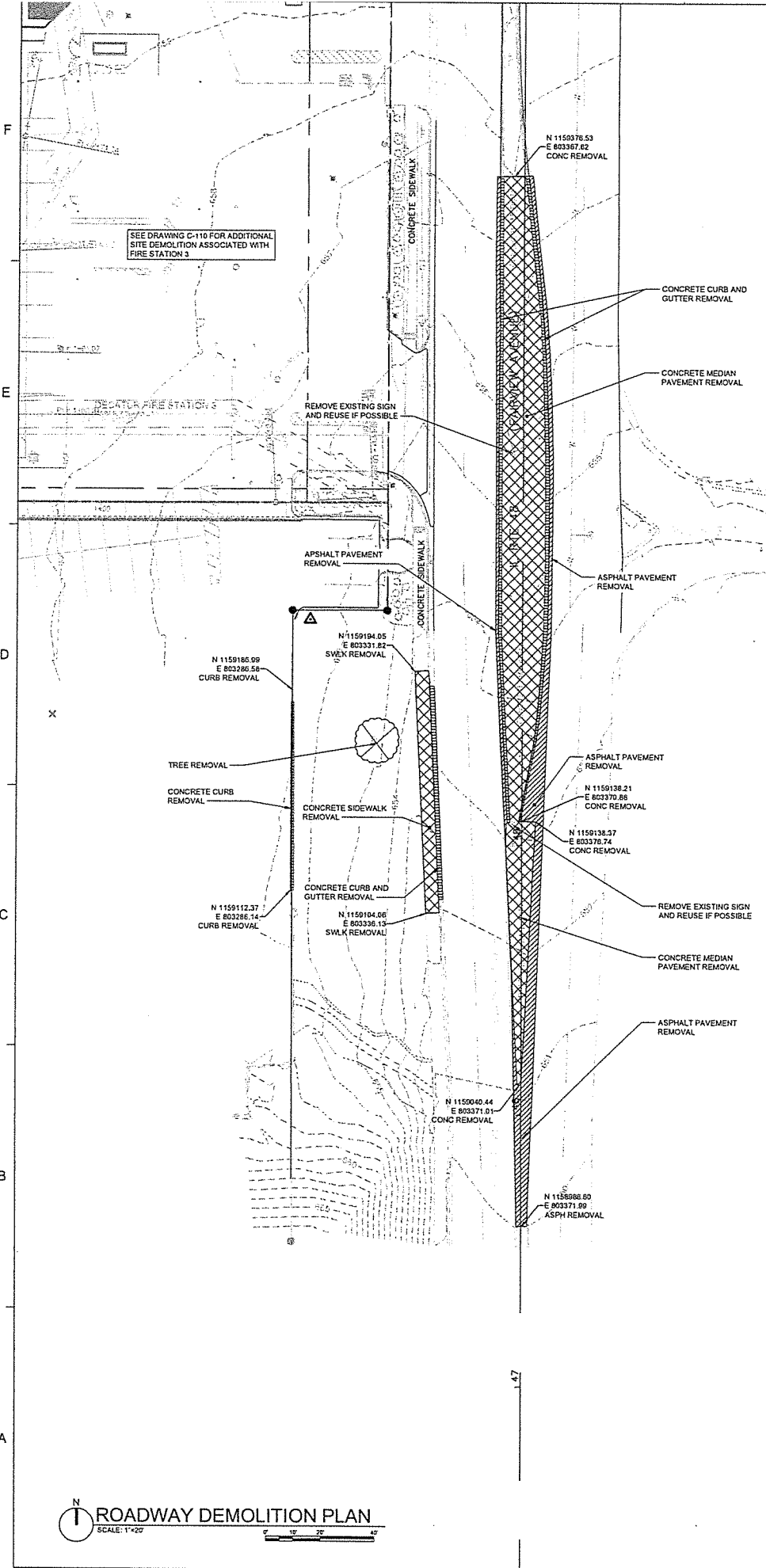
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

Note: All dimensions are in INCHES
(millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

FILE NAME :	USER NAME : staffennk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	FILE PTE.	SECTION	COUNTY	TOTAL SHEET NO.
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PLOT SCALE = 3.0000" = 1 IN.	CHECKED -	REVISED -							
PLOT DATE = 8/6/2010	DATE -	REVISED -							
				SCALE:	SHEET NO. 4 OF 4 SHEETS	STA.	TO STA.		
							CONTRACT NO.		
							ILLINOISIFIED, AID PROJECT		



DEMOLITION LEGEND	
	REMOVE EXISTING ASPHALT PAVEMENT AND BASE
	REMOVE EXISTING CONC PAVEMENT AND AGGREGATE BASE
	REMOVE EXISTING CONC CURB AND GUTTER AND AGGREGATE BASE
	DETECTABLE WARNING
	TREE REMOVAL

- GENERAL SITE DEMOLITION NOTES
1. REFER TO SITE DEMOLITION PLAN SHEET C-110 FOR GENERAL DEMOLITION NOTES.
 2. ALL SIGNS INDICATED ON THE ROADWAY DEMOLITION OR WITHIN THE DEMOLITION AREAS SHALL BE REMOVED, PROTECTED AND SET ASIDE ON SITE TO BE REUSED, IF POSSIBLE. FOR THE NEW SIGNS INDICATED ON THE PAVEMENT MARKING AND SIGNAGE PLAN C-132.

- GENERAL NOTES
1. REFER TO SITE GRADING PLAN C-120 FOR GENERAL NOTES.
 2. MEDIAN PEDESTRIAN CROSSINGS. REFER TO IDOT HIGHWAY STANDARD 424031-01
 3. PERPENDICULAR SIDEWALK RAMP WITH DETECTABLE WARNING. REFER TO IDOT HIGHWAY STANDARD 424001. TIPPED-OUT DEPRESSIONED CURB & GUTTER REFER TO IDOT HIGHWAY STANDARD 606001-07 AND HAS A CURB HEIGHT OF 12" MAX. FOR TIPPED-IN DEPRESSIONED CURB & GUTTER REFER TO IDOT HIGHWAY STANDARD 606001-07
 4. B-6 12 DEPRESSIONED CURB AND GUTTER. REFER TO IDOT HIGHWAY STANDARD 606001-07
 5. TIPPED-IN GUTTER. SEE DETAIL 3/C-132
 6. TIPPED-OUT GUTTER. SEE DETAIL 3/C-132
 7. SIDE CURB. REFER TO IDOT HIGHWAY STANDARD 424006-04 SIDE CURB DETAIL.

Dewberry

DEWBERRY ARCHITECTS INC.
#184.000358-0001

DEWBERRY ENGINEERS INC.
#184005007-0006

401 SW WATER ST
SUITE 701
PEORIA, IL 61602
309.262.8005

sda
Schoppe Design Associates, Inc.

CITY OF DECATUR, ILLINOIS
DECATUR FIRE STATION 3
855 NORTH FAIRVIEW AVE., DECATUR, ILLINOIS
BID SET

SEAL

6/28/2020
Exp. 11/30/2021

KEY PLAN

SCALE

1"=20'

REVISIONS		
NO.	DESCRIPTION	DATE
1	DOT REVISIONS	6/19/2020
2	ADDENDUM 1	6/22/2020
1	SITE PLAN REVIEW	10/26/2020

DRAWN BY MDS
APPROVED BY MPB
CHECKED BY MPB
DATE 05/21/2020

TITLE

ROADWAY
DEMOLITION
& REPAIR
PLAN

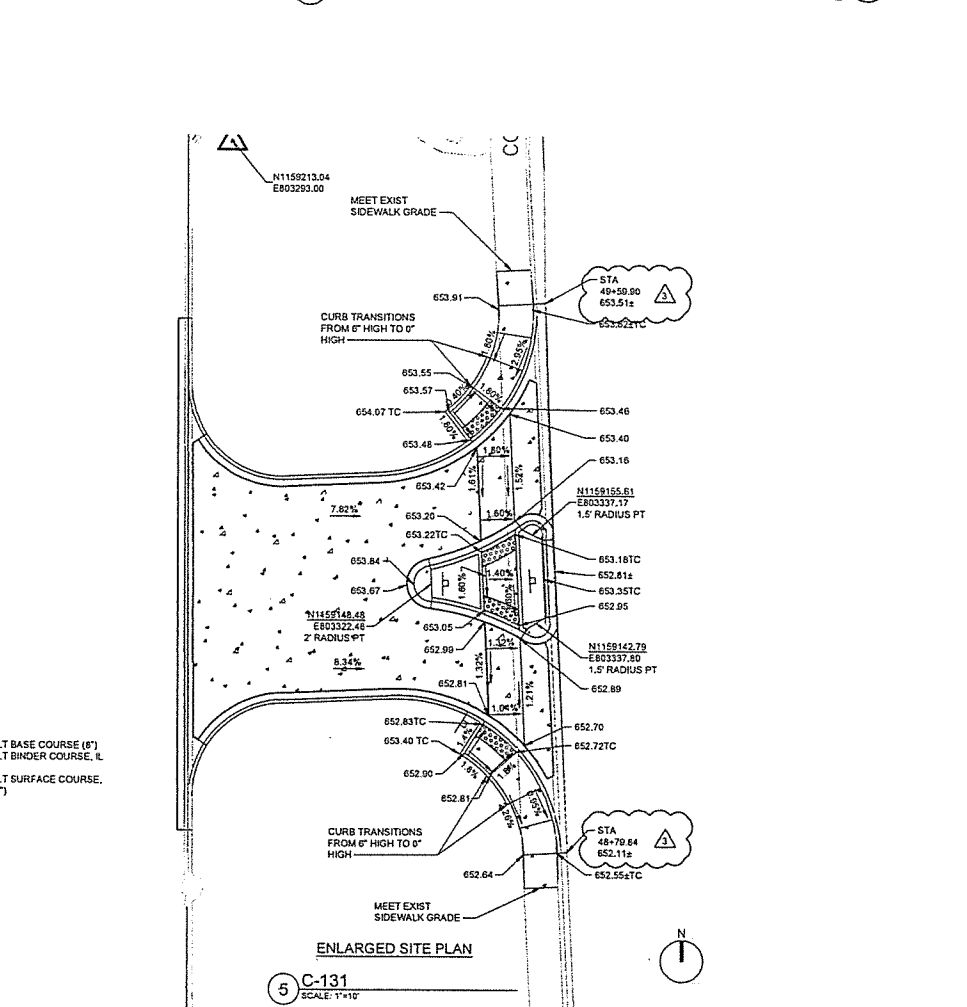
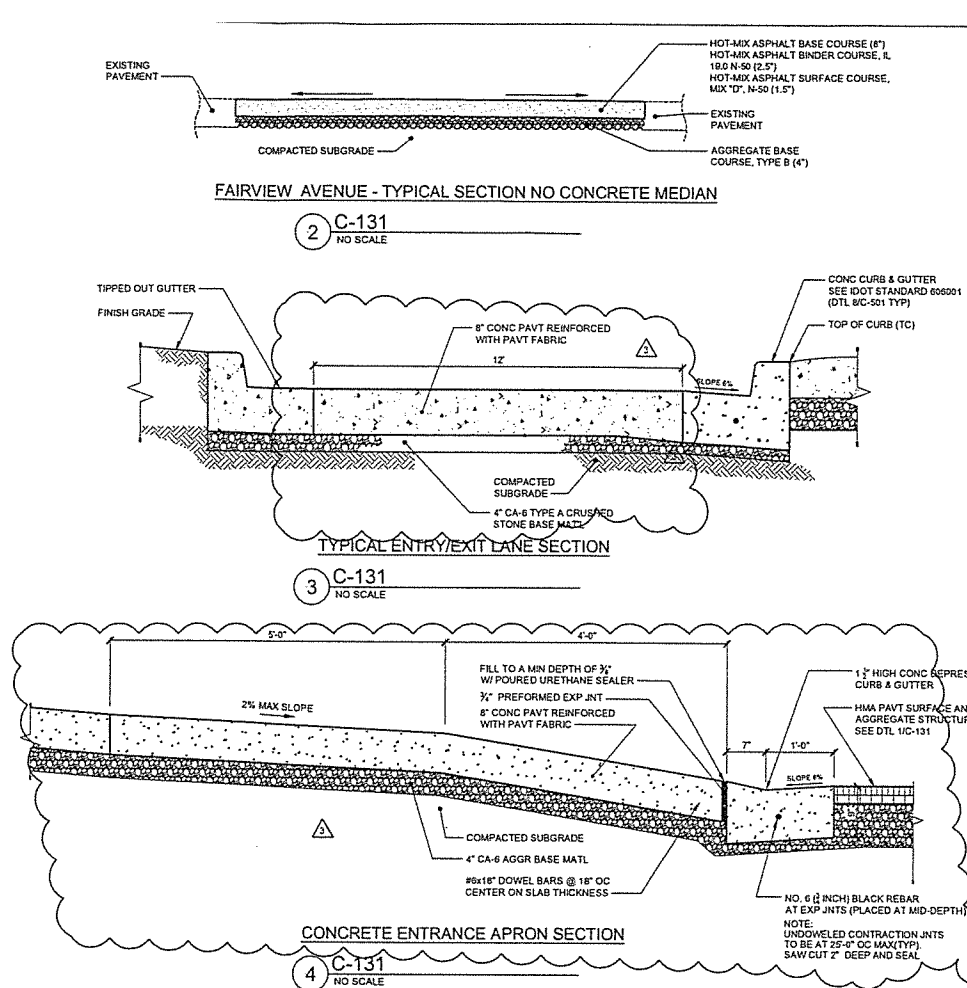
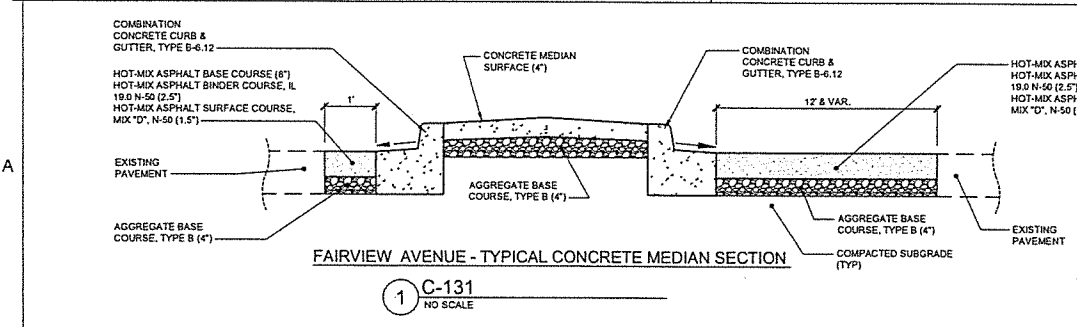
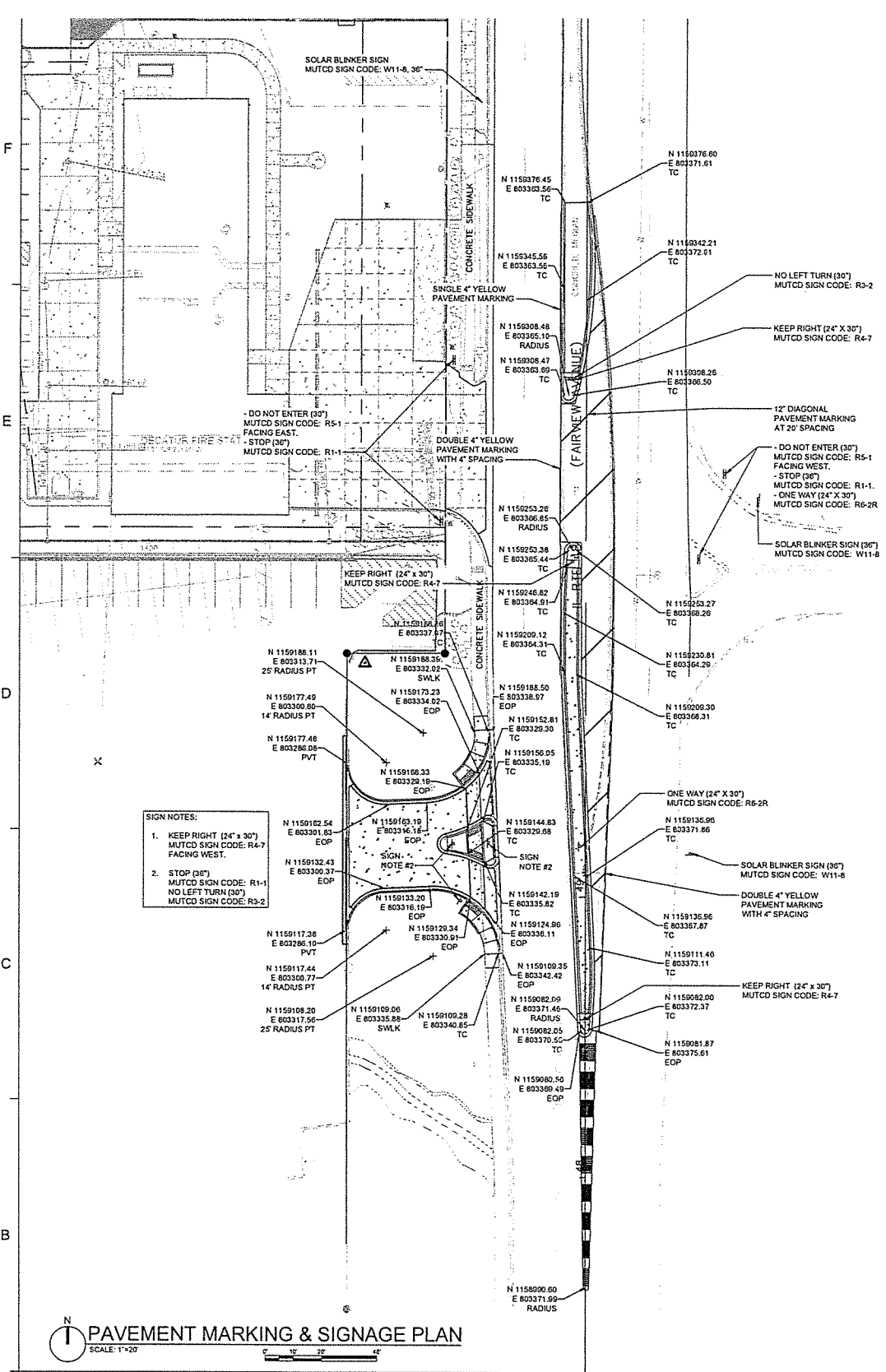
PROJECT NO. 52111233

C-131

SHEET NO.

Call Before You Dig
JULIE
800-CALL SYSTEM
Simply Call 811

AT LEAST 48 HOURS PRIOR TO EXCAVATION
CONTACT J.U.L.I.E. AT 1-800-892-0123



- GENERAL NOTES**
- 1) WORK WITHIN IDOT RIGHT-OF-WAY TO BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE IDOT STANDARDS AS NOTED BELOW
 - PAVEMENT FABRIC (420701)
 - PERPENDICULAR CURB RAMP FOR SIDEWALKS (424001)
 - ENTRANCE PEDESTRIAN CROSSINGS (424020-01)
 - MEDIAN PEDESTRIAN CROSSINGS (424031-01)
 - CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER (806001)
 - TYPE B GUTTER (ENTRANCE) (806201-02)
 - PC CONCRETE ISLANDS AND MEDIANS (606001)
 - CORRUGATED PC CONCRETE MEDIANS (606006)
 - URBAN LANE CLOSURE, MULTILANE 2W WITH BIDIRECTIONAL LEFT TURN LANE (701002)
 - TRAFFIC CONTROL DEVICES (701001)
 - METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS (720011)
 - 2) ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC

Dewberry

DEWBERRY ARCHITECTS INC.
#184.000358-0001

DEWBERRY ENGINEERS INC.
#184005007-0006

401 SW WATER ST
SUITE 701
PEORIA, IL 61602
309.252.6262

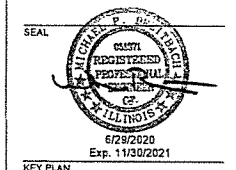
sda
Schaepp Design Associates, Inc.

CITY OF DECATUR, ILLINOIS

DECATUR FIRE STATION 3

855 NORTH FAIRVIEW AVE, DECATUR, ILLINOIS

BID SET



SCALE

REVISIONS		
NO.	DESCRIPTION	DATE
1	1. SITE PLAN REVIEW	02/20/2020
2	2. ADDENDUM 1	03/30/2020
3	3. IDOT REVISIONS	02/26/2020

DRAWN BY: MDS

APPROVED BY: MPB

CHECKED BY: MPB

DATE: 05/21/2020

TITLE

PAVEMENT MARKING & SIGNAGE PLAN

PROJECT NO. 50111233

C-132

SHEET NO.